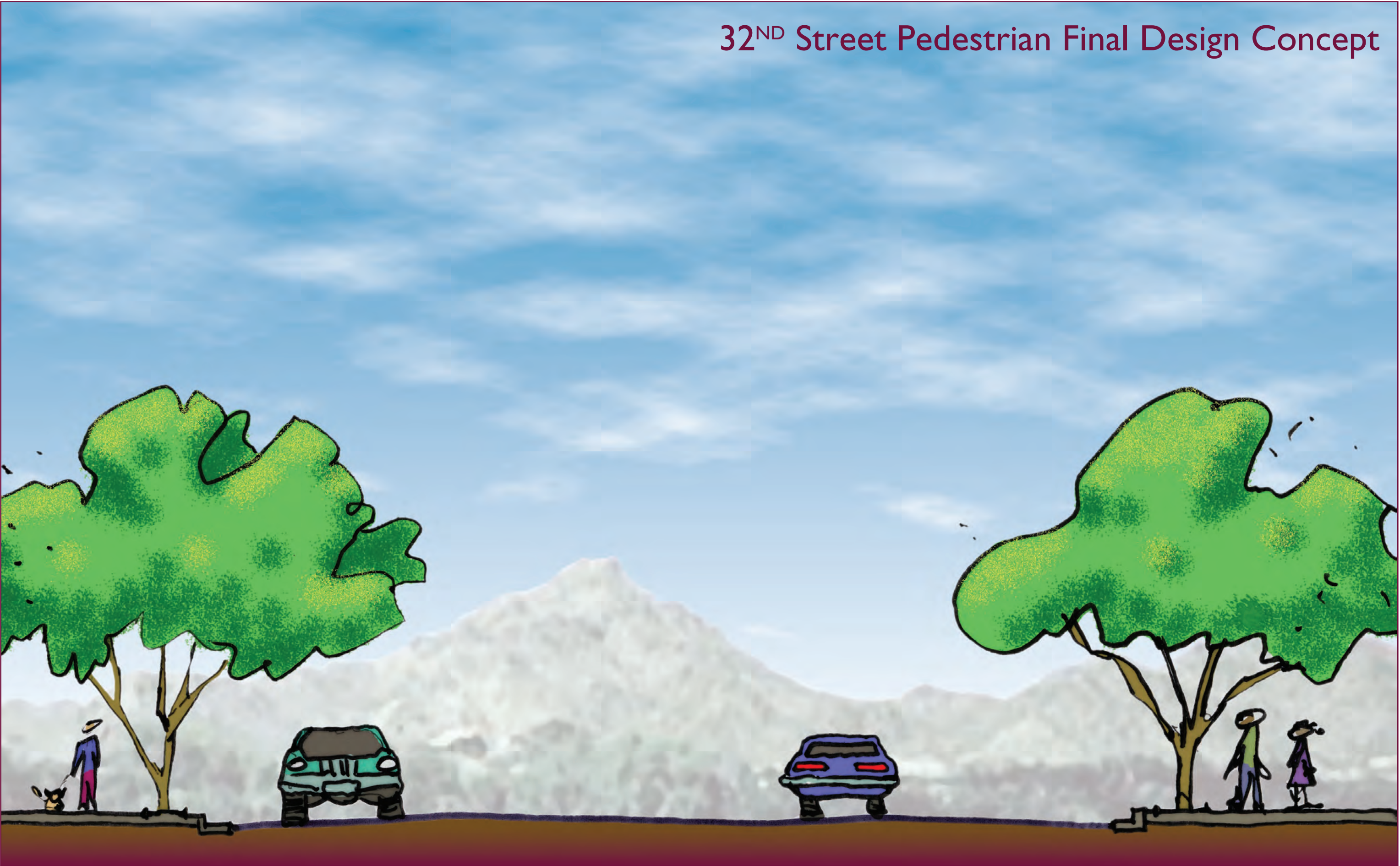


# 32<sup>ND</sup> Street Pedestrian Final Design Concept





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Appendix A

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Background

The 32nd Street Pedestrian Study began as a way to improve existing pedestrian linkages to the Light Rail Transit (LRT) Platform that was proposed for Washington Street at 32nd Street. The study area encompassed Roosevelt Street to Washington Street and 29th Street to 34th Street. Many pedestrian improvement alternatives were investigated. Some of these included 32nd Street, the abandoned Salt River Valley Canal, 29th Street, 30th Street, 32nd Place and Van Buren Street. After observation of the neighborhood pedestrian circulation, these routes were deemed as the most used.

After the LRT Platform was moved further east on Washington Street, the City of Phoenix and MAG agreed that the need for pedestrian improvements to this neighborhood were still necessary.

A survey was prepared that included a map of the study area. Residents were asked to show, on the map, the routes they used most often. Using information from the survey, a matrix was prepared listing the various circulation routes. In conjunction with the survey and other resource material, the City and MAG team representatives were asked to rate the importance of each route. 32nd Street was voted as the more necessary circulation improvement.

32nd Street provides a central pedestrian circulation through the neighborhood. It provides a route to the bus stops, schools, shopping and popular short cuts such as the abandoned Salt River Project (SRP) canal. The abandoned canal begins at 32nd Street, just north of Fillmore, and runs at a 45°-angle north to 30th Street south of Roosevelt. The intersection of 32nd Street and Van Buren Street is considered the “Heart of the Community”. An abandoned minimum-security prison currently occupies the northwest corner. This may change soon. Corazon de Oro, Wilson Community Development Corporation and Fomento Firme Association, Inc. are working on plans for redevelopment of this corner. The Wilson Renaissance Project is proposed to provide a Cultural Center, Community Center with an Amphitheater, swimming pool and ballfield. Housing and retail are also proposed for the site. Safe pedestrian access through the community is a key part of their proposal.

Safety was a significant concern and was addressed by the majority of those surveyed. Drug trafficking and prostitution are rampant on the streets and the residents are irate. This neighborhood is judged unfairly by the illicit activities that occur on the perimeter streets. Within the study area are residential neighborhoods, schools and

a church. It has strong community leaders and dedicated residents. Most of the residents walk out of necessity and want to do so safely, with ample shade and protected from vehicles.

The MAG Pedestrian Area Policies and Design Guidelines offers guidance in planning improvements for the pedestrian. The Guidelines provide three levels of pedestrian areas. These levels are used to evaluate the subject study area

The pedestrians in this neighborhood walk out of necessity, for recreation and to socialize. They do not feel safe from crime, vehicles and sidewalk hazards. 32nd Street, with its’ 6 lanes of high-speed traffic, narrow shade-less sidewalks and numerous curb cuts illustrates that vehicles have precedence over pedestrians. Traffic and pedestrian volumes are moderate to heavy. Celebrity Theater and Christies provide tourist draw, however, these visitors drive in for their experience and then leave.

Other areas within the Valley with similar characteristics to name a few include Cave Creek Road at 7th Street north to the 101 Freeway, McDowell road at 32nd Street east to the 51 Freeway, 16th Street at Indian School Road south to Washington, and the Grand Avenue Corridor. There are many more with the same or similar pedestrian level as this 32nd Street Study. This study could be used as a supplemental guide to pedestrian improvements for these locations.



Existing Conditions

A plan for the existing 32nd Street road and sidewalk conditions was prepared with on-site analysis and as-built drawings provided by the City of Phoenix.

Currently, 32nd Street has three northbound lanes, two southbound lanes and a center turn lane that runs the

length of the study area. 32nd Street from Roosevelt to just north of Van Buren has an 80-foot right-of-way which tapers to 84 feet south through the intersection. The 80 foot right-of-way section contains 2 feet of curb and gutter for each side, two 5 foot sidewalks and a 64-foot wide roadway. From Van Buren to Washington the right-of-way widens to 84 feet. This right-of-way contains 65 feet of roadway, 2 feet on each side for curb and gutter and two 5-foot sidewalks. There are four bus stops: the southwest



corner of 32nd Street and Roosevelt, each side of 32nd south of Fillmore and the northeast corner of 32nd Street and Van Buren. There are no bus stops south of Washington on 32nd Street. The bus stop at Roosevelt provides a bench and trash receptacle. The stop on the westside of 32nd at Fillmore has a shelter, bench and trash receptacle. The bus stop on the eastside of 32nd Street north of Van Buren provides a shelter with kiosk advertising, a bench and trash receptacle, which are owned and maintained by a private enterprise. The stop on the eastside of 32nd at Fillmore provides a bus stop sign. There are many curb cuts, some to empty lots or abandoned businesses. Many of the existing operations have two or more curb cuts. There are few trees and very little shade.

A set of plans exhibiting the existing conditions has been provided in this report.

Preferred Alternative

Four Alternative Designs were presented to the City of Phoenix and MAG. From these four, a hybrid was developed and determined to be the Preferred Alternative.

The Preferred Alternative will reduce the existing six traffic lanes to five traffic lanes; two northbound lanes, two southbound lanes and a shared center lane. The 84

foot right-of-way south of Van Buren will typically provide 9 foot sidewalks each on side, 4 foot planters with tree grates on each side, 1 foot header on each side, 2 foot curb and gutter on each side, one 10.58 foot southbound lane, one 10 foot southbound lane, a 10 foot shared center lane, one 10 foot northbound lane, and one 10.58 foot northbound lane. The 80 foot right-of-way north of Van Buren will typically have 7 foot sidewalks on each side, 4 foot planters on each side, 1 foot header on each side, 2 foot curb and gutter on each side, one 10.58 foot southbound lane, one 10 foot southbound lane, a 10 foot shared center lane, one 10 foot northbound lane, and one 10.58 foot northbound lane. The planters for each right-of-way width are proposed to contain trees in tree grates. The trees within the sight visibility triangles will be pruned to maintain the City of Phoenix required 6’8” minimum clearance below the canopy. The tree species will be those allowed by the Department of Water Resources Low Water Use Plant List. Curb cut elimination or reduction to one curb cut is proposed for the abandoned and unused sites and reduction of curb cuts is proposed for sites containing more than two.

The reduction in road width will impact the existing infrastructure such as; storm drains, catch basins, traffic signal and streetlights.

Based on the Preferred Alternative, the City of Phoenix Street Transportation Department prepared an engineering base sheet for the length of 32nd Street, Roosevelt to Washington. The base sheet provided for the Preferred Alternative sidewalks, planters, traffic lanes and the necessary tapering to the existing right-of-way conditions at Roosevelt and Washington. Because of the taper of the road to existing conditions the infrastructure at Roosevelt and Washington will remain the same. The existing traffic signals may need to be relocated at the intersection of 32nd Street and Van Buren, as this is the only signaled intersection affected by the traffic lane reduction. The roadway tapering to existing conditions is approximately 200 feet south of the centerline of Roosevelt and 200 feet north of the centerline of Washington. The streetlights, grades, catch basins and storm drains will be affected between these points. More detailed, engineered analysis will be necessary to establish the extent of these relocations.

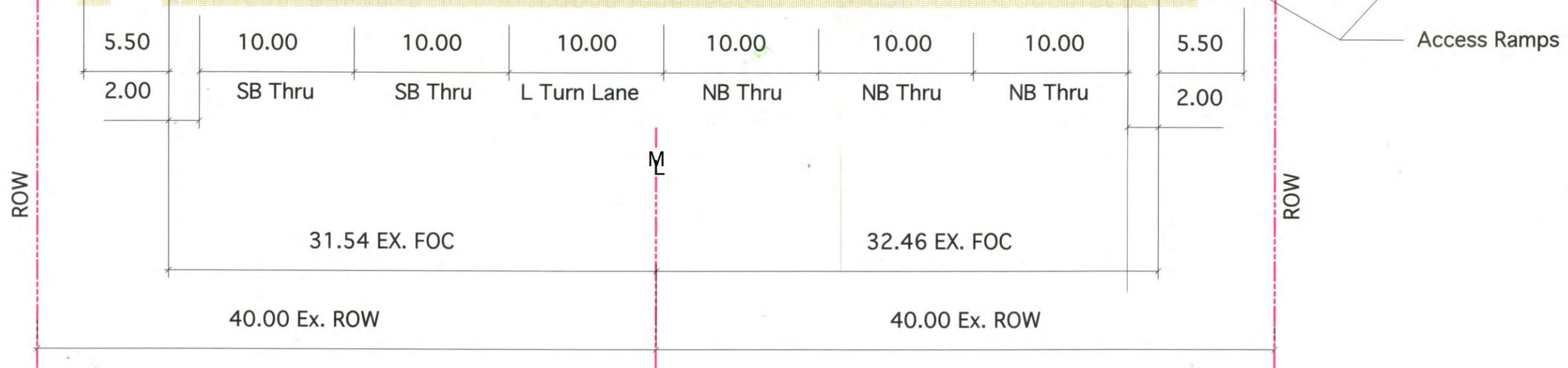
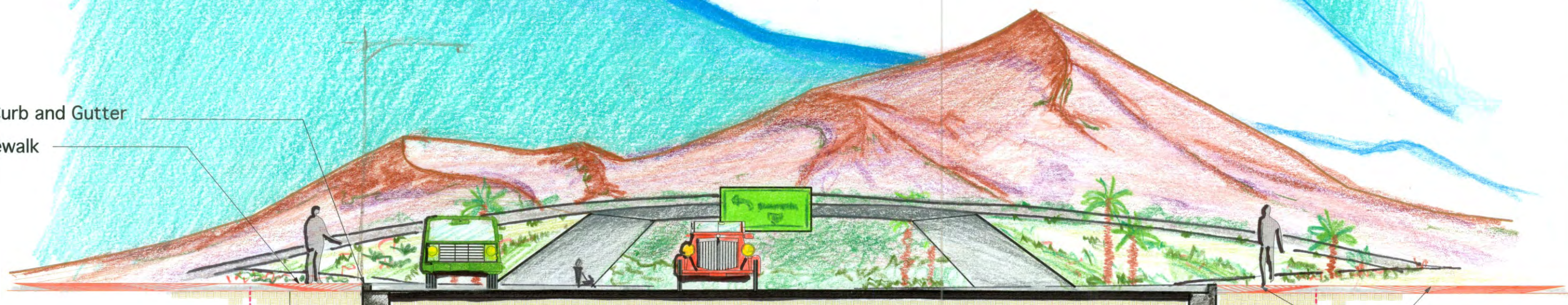
The pedestrian improvements will not only impact the residents; existing businesses and the proposed Wilson Renaissance Project can share in the added amenities offered in this streetscape enhancement. Interconnected pedestrian links within communities can become instrumental in developing a stronger sense of place within the City’s urban core.



SHERMAN GROUP INC  
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2' Curb and Gutter  
Sidewalk



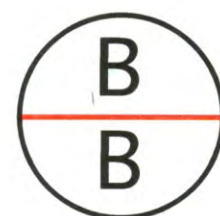
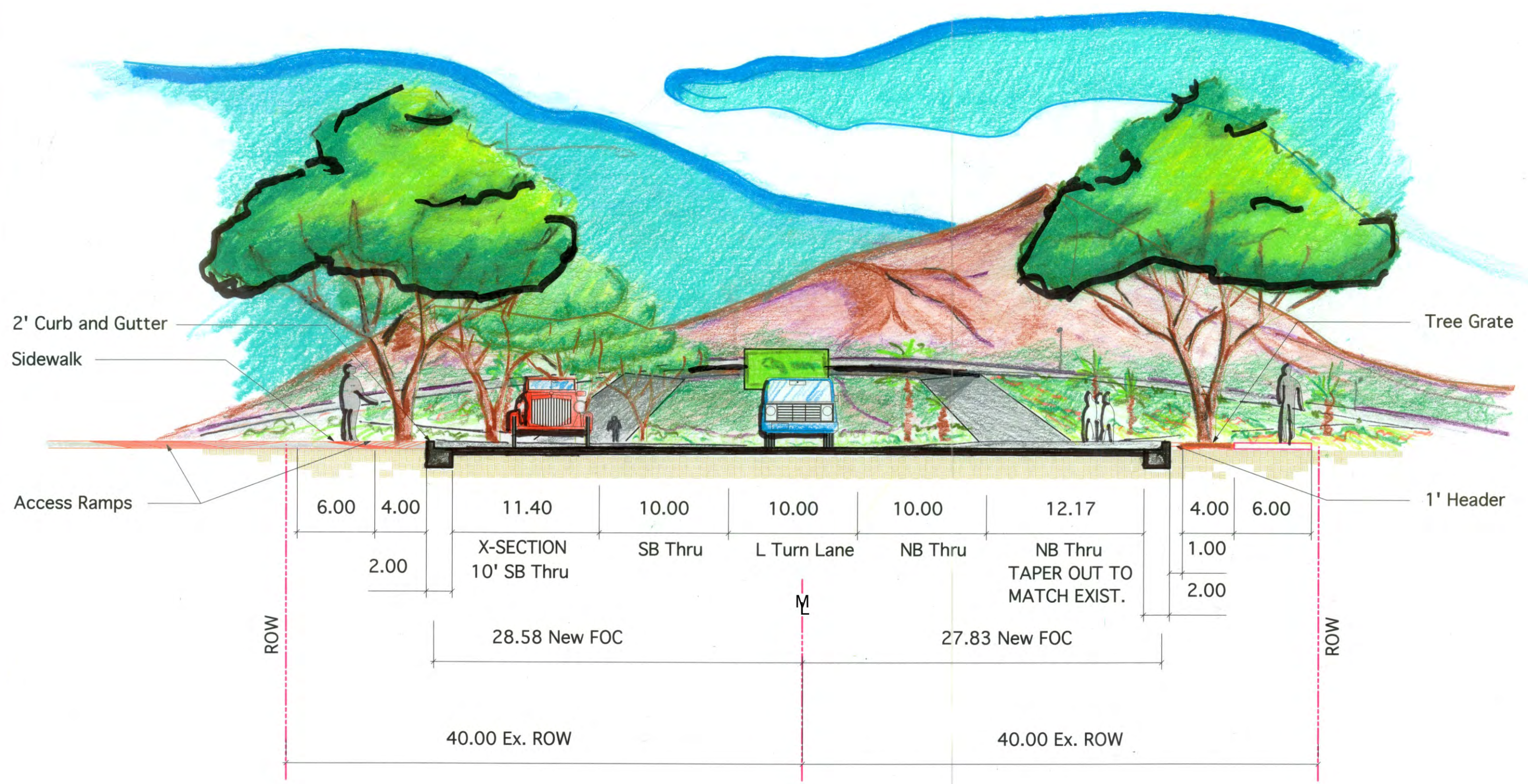
A
A

## SECTION A-A

## E. ROOSEVELT ST.

Dimensions provided by City of Phoenix Street Transportation Department

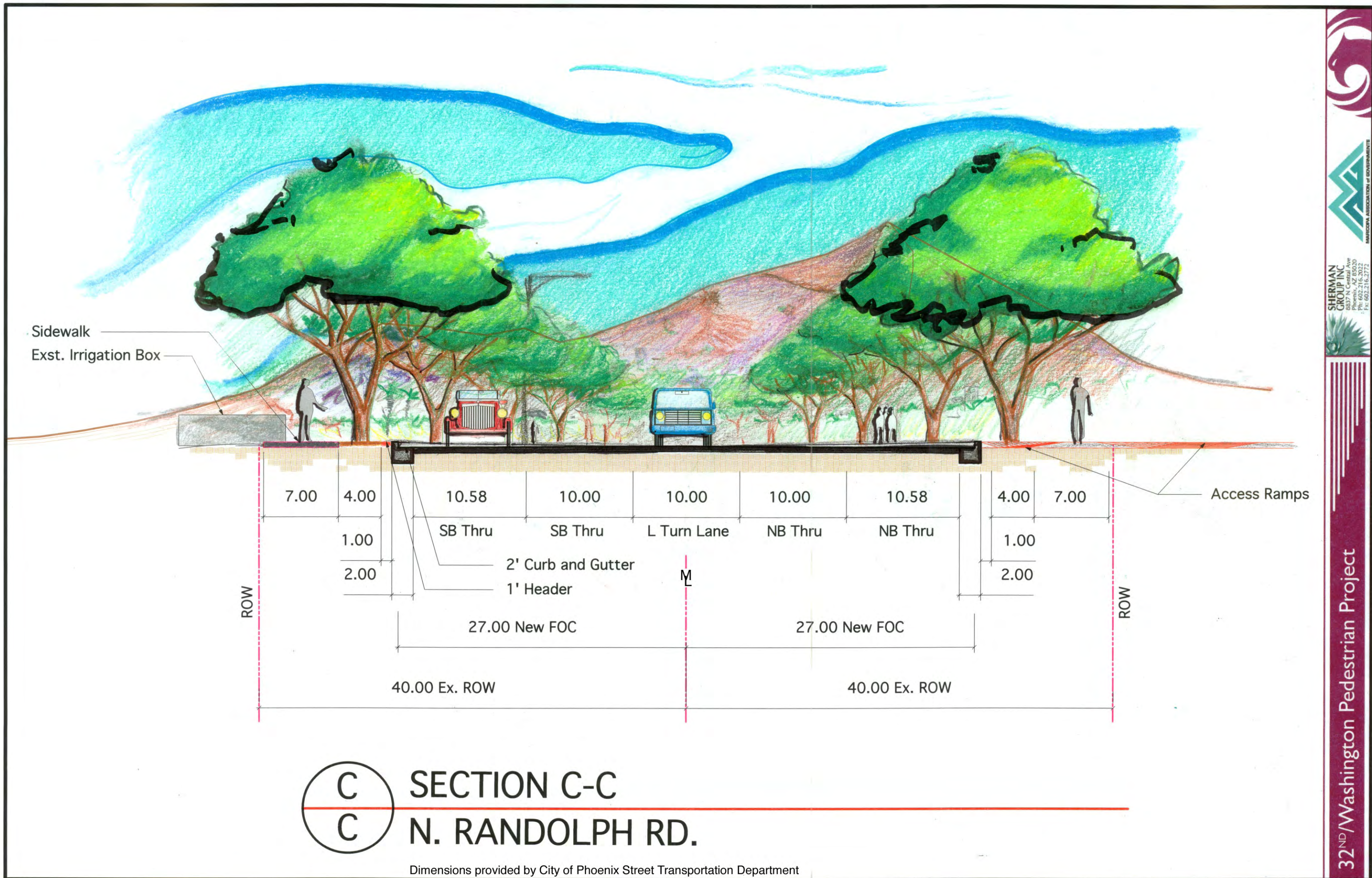




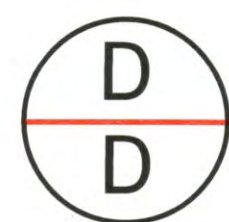
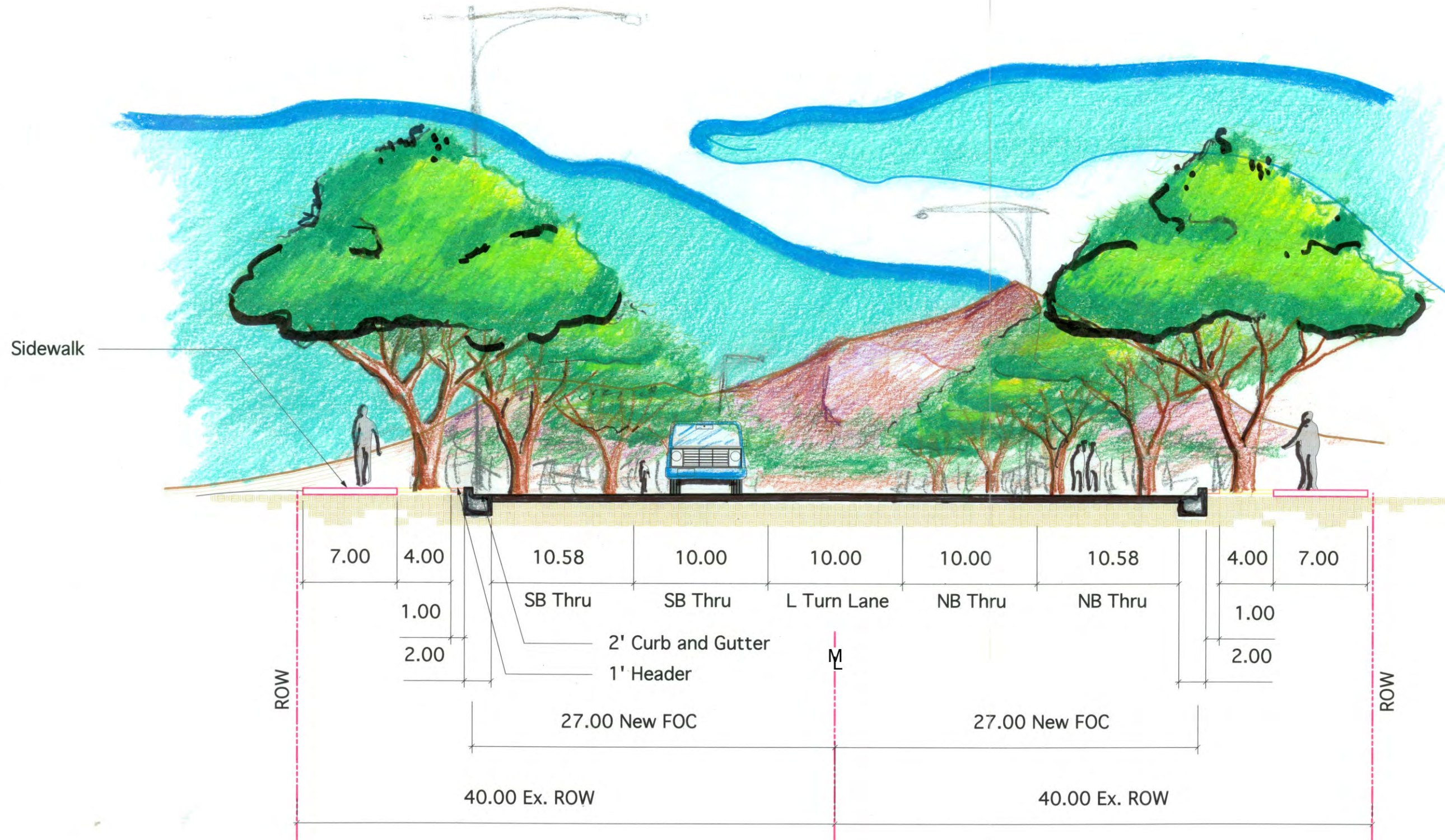
# SECTION B-B E. GARFIELD ST.

Dimensions provided by City of Phoenix Street Transportation Department







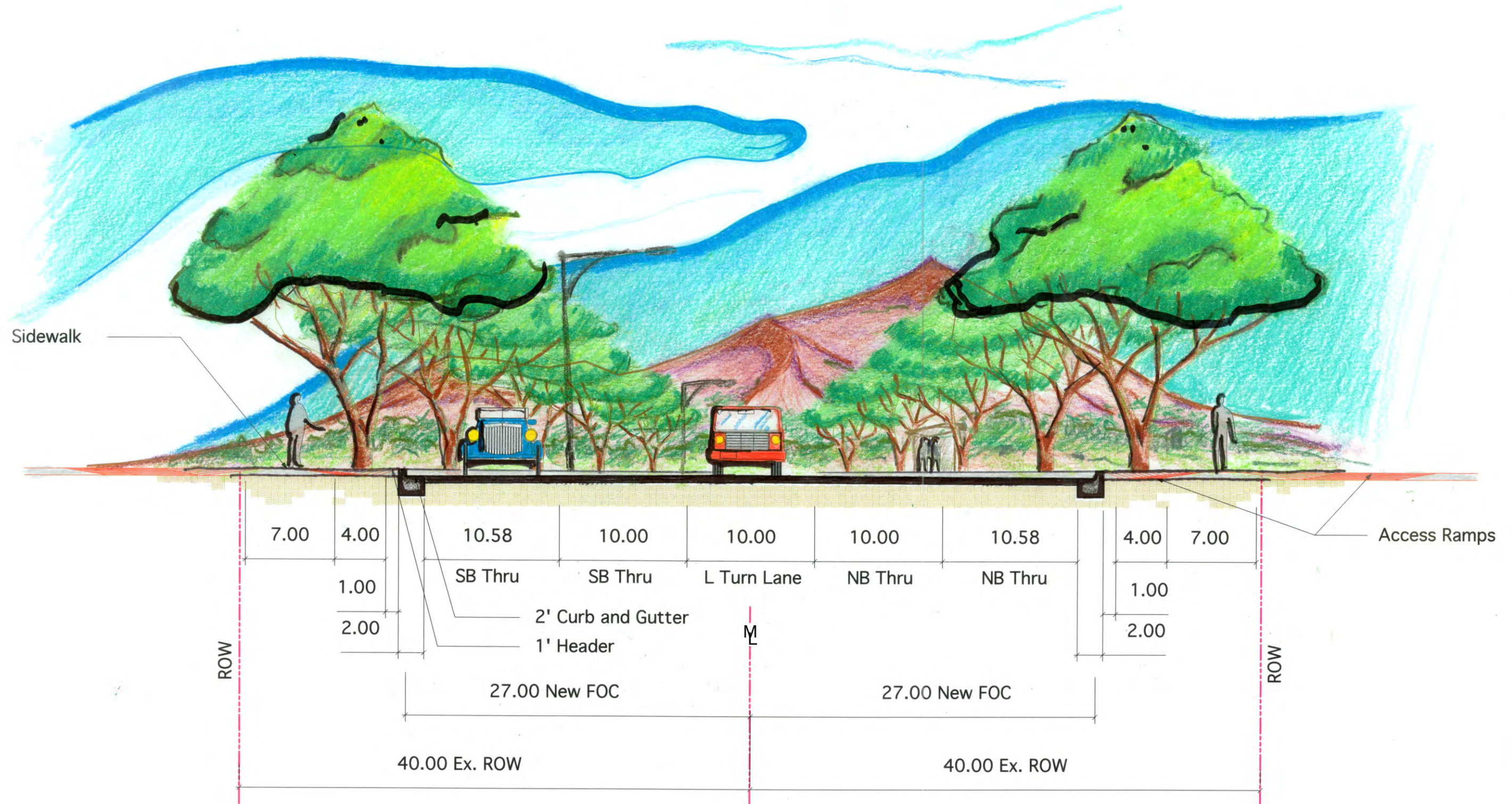


# SECTION D-D

## E. FILMORE ST. - VAN BUREN ST.

Dimensions provided by City of Phoenix Street Transportation Department





## SECTION E-E E.VANBUREN ST.

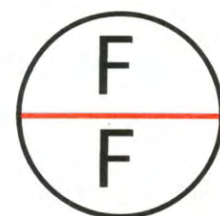
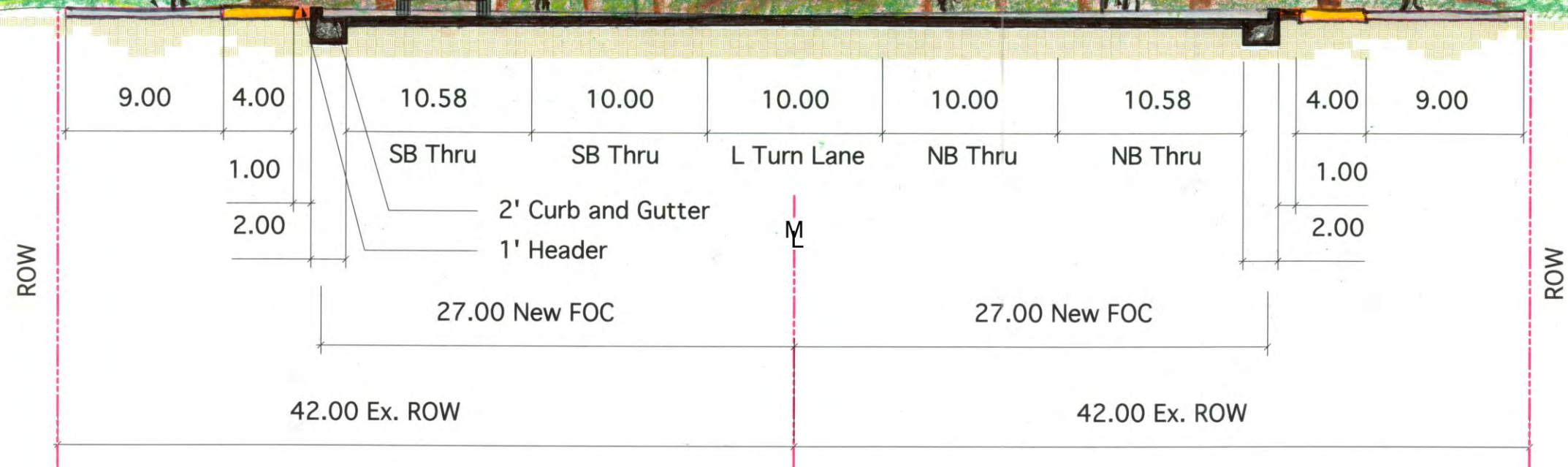
Dimensions provided by City of Phoenix Street Transportation Department



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8837 N Central Ave  
Phoenix, AZ 85020  
Ph: 602.216.2022  
Fax: 602.216.2772



Sidewalk



SECTION F-F

E. VANBUREN ST. - E. WASHINGTON ST.

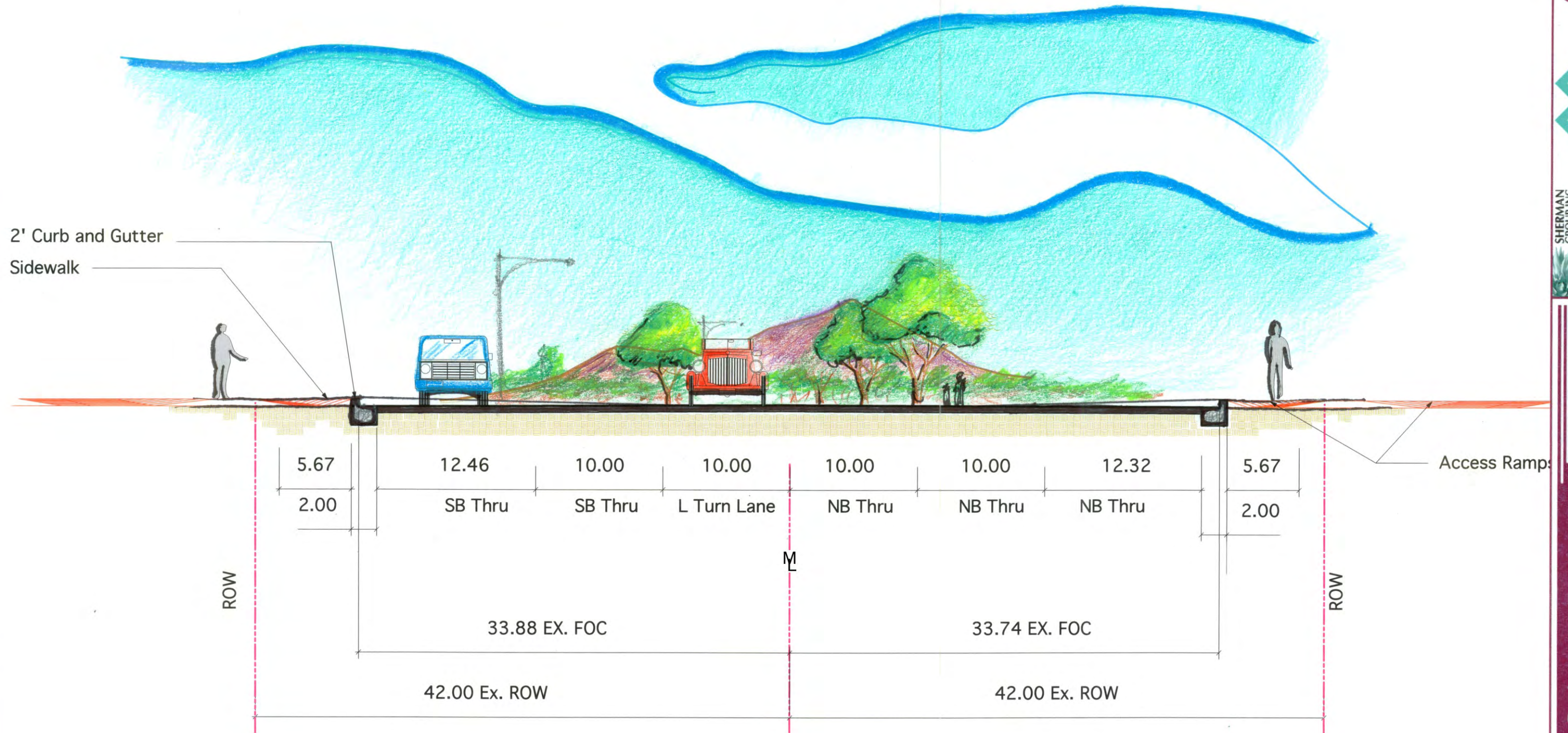
Dimensions provided by City of Phoenix Street Transportation Department



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F: 602.216.2772

32<sup>ND</sup> Washington Pedestrian Project





**SECTION G-G**  
**E. WASHINGTON ST.**

Dimensions provided by City of Phoenix Street Transportation Department



**SHERMAN GROUP INC.**  
8837 N Central Ave  
Phoenix, AZ 85020  
P: 602.216.2772  
F: 602.216.2773



**32<sup>ND</sup> Washington Pedestrian Project**



LIMITS OF PROJECT

0+00

E ROOSEVELT ST

MATCH TO EXISTING AT  
STA 01+84.91

MATCH TO EXISTING AT  
STA 02+02.96

E GARFIELD ST

E GARFIELD ST

MATCH LINE STA 4+50.00  
SEE DWG NO. LS-2

NOTE:  
ALL TREES IN VISIBILITY TRIANGLES TO BE PRUNED TO ALLOW A MINIMUM OF 6' 8" CLEARANCE BELOW THE CANOPY  
SEE CROSS SECTIONS FOR LANE WIDTH, SIDEWALK AND PLANTER DIMENSIONS.

33'X33' VISIBILITY  
TRIANGLE TYP.  
10'X20' VISIBILITY  
TRIANGLE TYP.  
PROPOSED  
DRIVEWAY TYP.

PEDESTRIAN RAMP TYP.

DECORATIVE HEADER

EXISTING CURB AND  
GUTTER

PROPOSED CATCH  
BASIN TYP.

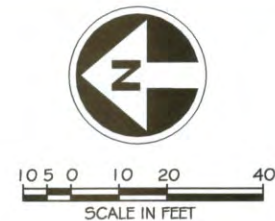
32ND STREET

RELOCATED STREET  
LIGHT TYP.

EXISTING ROW  
DECORATIVE HEADER

SHADE TREE  
NEW SIDEWALK  
EXISTING FIRE HYDRANT

12'X4' TREE GRATE

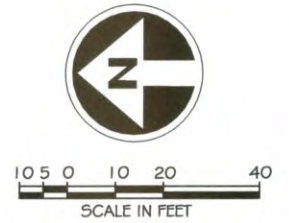
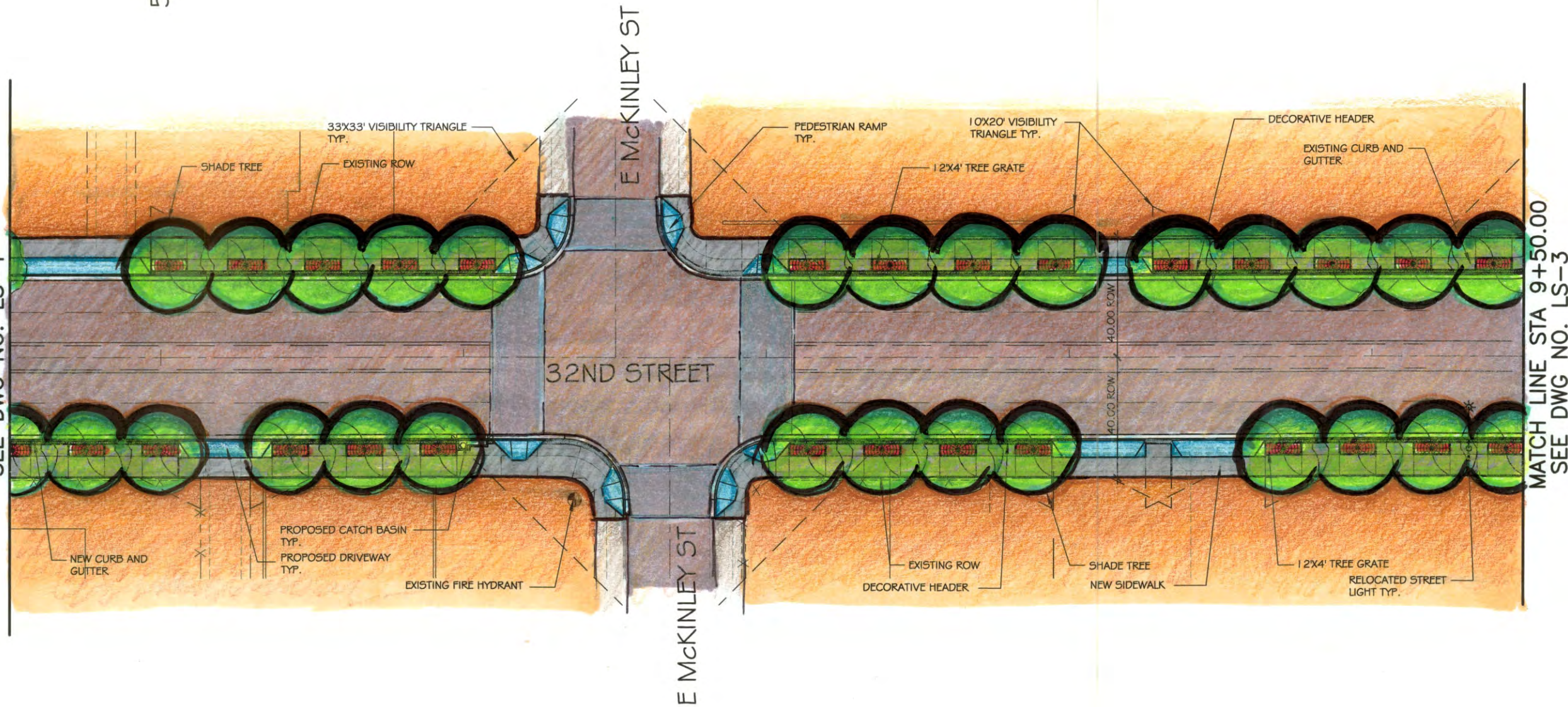


SHEET LS-1  
CONCEPTUAL LANDSCAPE PLAN  
32ND STREET  
STA 0+50.00 TO STA 4+50.00



MATCH LINE STA 4+50.00  
SEE DWG NO. LS-1

5+00



NOTE:  
ALL TREES IN VISIBILITY TRIANGLES TO BE PRUNED TO ALLOW A MINIMUM OF 6' 8" CLEARANCE BELOW THE CANOPY  
SEE CROSS SECTIONS FOR LANE WIDTH, SIDEWALK AND PLANTER DIMENSIONS.



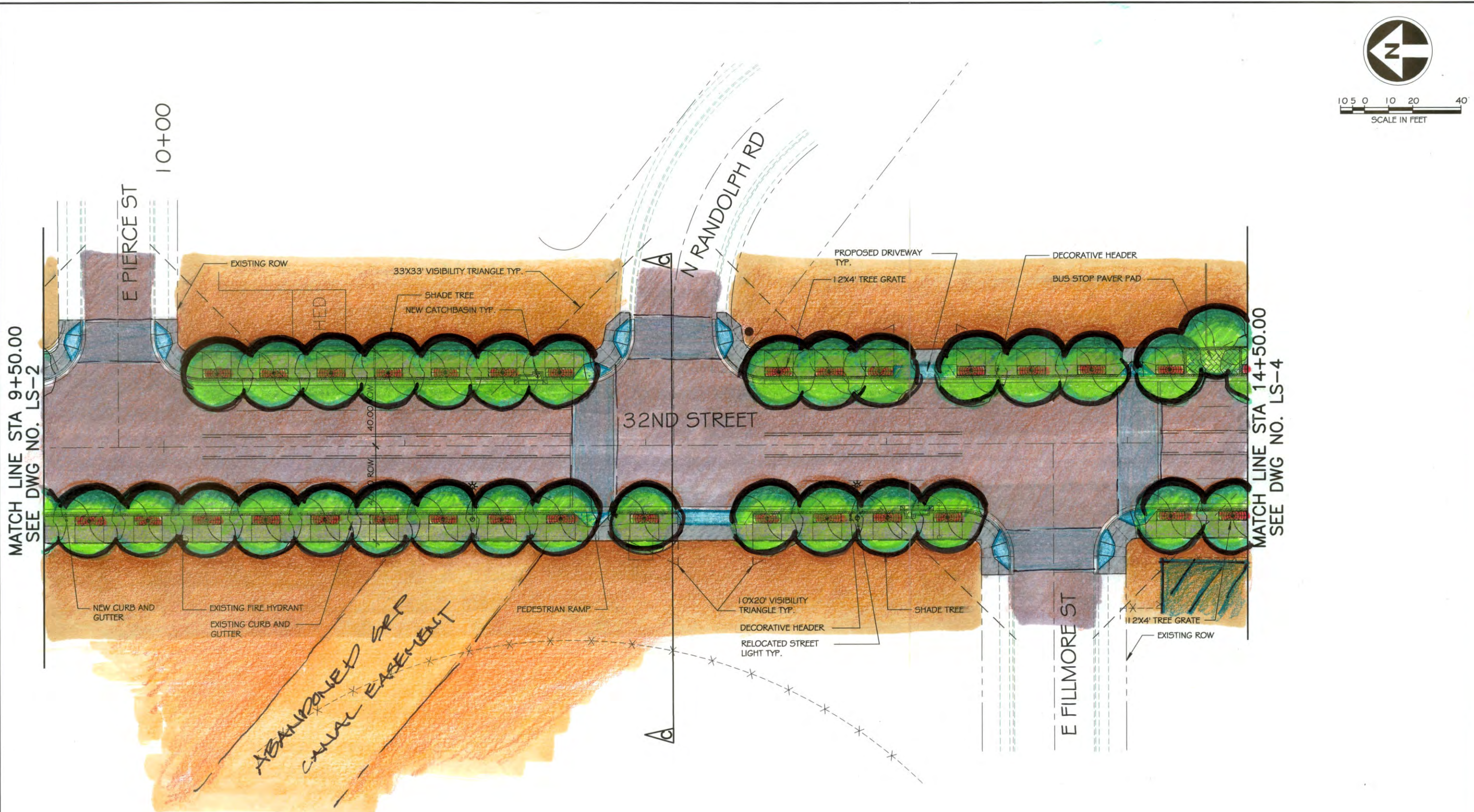
SHEET LS-2  
CONCEPTUAL LANDSCAPE PLAN  
32ND STREET  
STA 4+50.00 TO STA 9+50.00

**32<sup>ND</sup>/Washington Pedestrian Project**

**SHERMAN GROUP INC**  
8037 N Central Ave  
Phoenix, AZ 85028  
PH: 602-276-9020  
FX: 602-276-2772

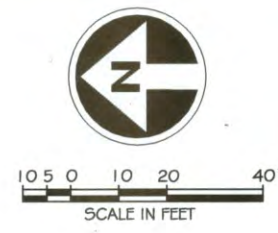
MARICOPA ASSOCIATION OF ENGINEERS





MATCH LINE STA 9+50.00  
SEE DWG NO. LS-2

MATCH LINE STA 14+50.00  
SEE DWG NO. LS-4



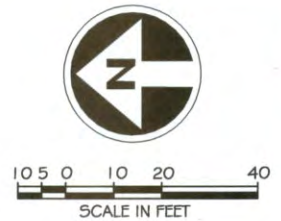
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SEE CROSS SECTIONS FOR LANE WIDTH, SIDEWALK AND PLANTER DIMENSIONS.

NOTE:  
FOR SECTIONS SEE SHEET LS-9



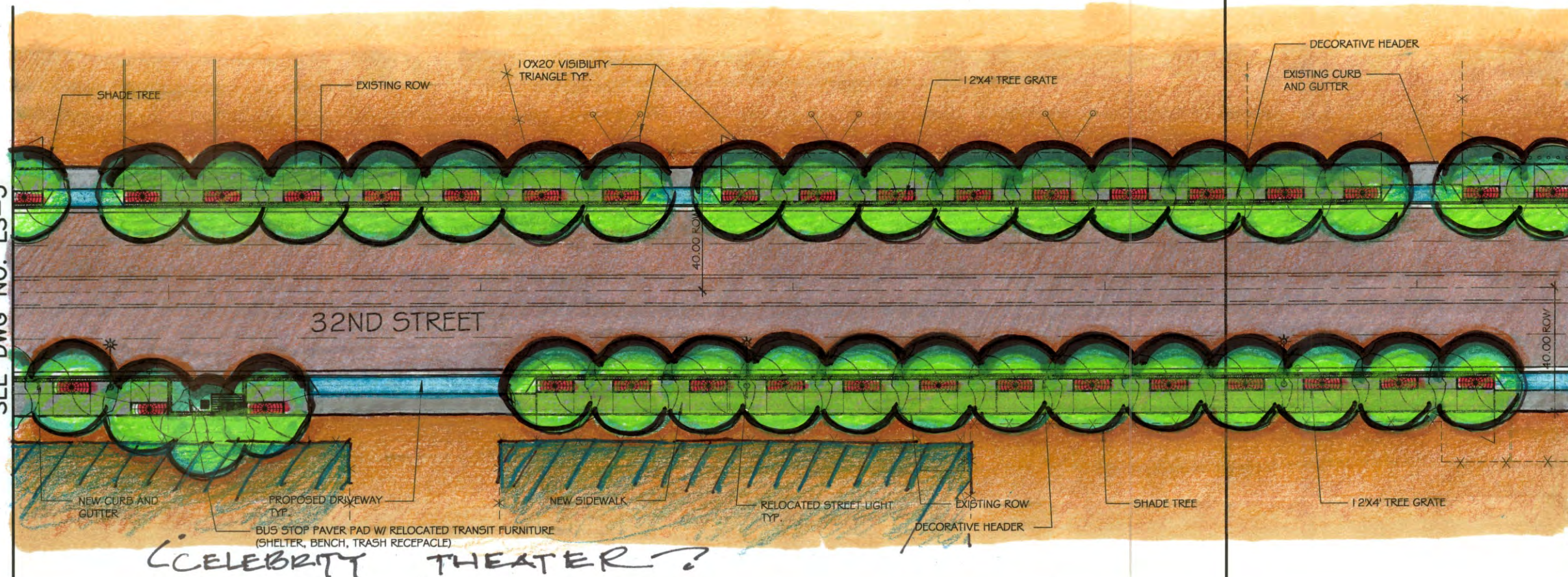
SHEET LS-3  
CONCEPTUAL LANDSCAPE PLAN  
32ND STREET  
STA 9+50.00 TO STA 14+50.00





MATCH LINE STA 14+50.00  
SEE DWG NO. LS-3

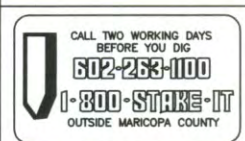
15+00



MATCH LINE STA 19+50.00  
SEE DWG NO. LS-5

NOTE:  
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SEE CROSS SECTIONS FOR LANE WIDTH, SIDEWALK AND PLANTER DIMENSIONS.

NOTE:  
FOR SECTIONS SEE SHEET LS-9

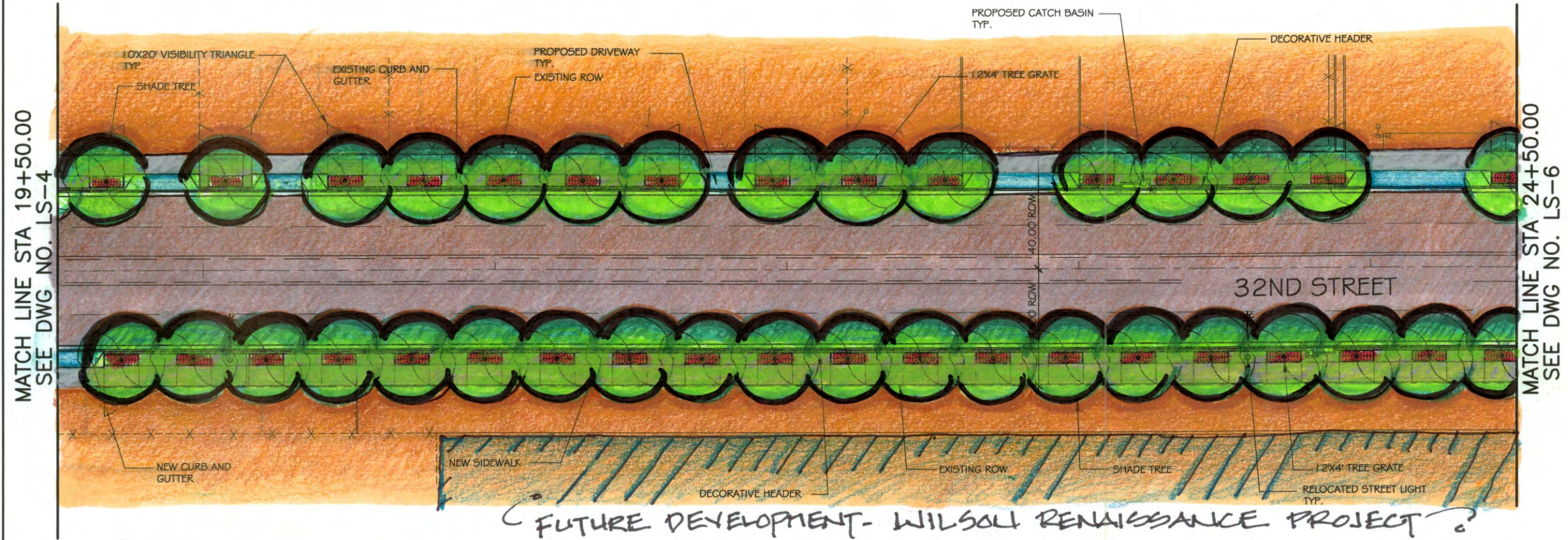
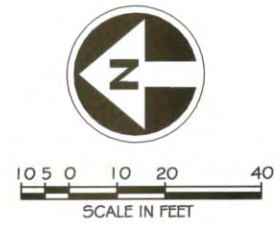


SHEET LS-4  
CONCEPTUAL LANDSCAPE PLAN  
32ND STREET  
STA 14+50.00 TO STA 19+50.00

SHERMAN  
GROUP INC  
10000 W. Camelback Road  
Phoenix, AZ 85020  
Ph: 602.216.2022  
Fx: 602.216.2772

32<sup>ND</sup>/Washington Pedestrian Project





NOTE:  
ALL TREES IN VISIBILITY TRIANGLES TO BE PRUNED TO ALLOW A MINIMUM OF 6' 8" CLEARANCE BELOW THE CANOPY  
SEE CROSS SECTIONS FOR LANE WIDTH, SIDEWALK AND PLANTER DIMENSIONS.

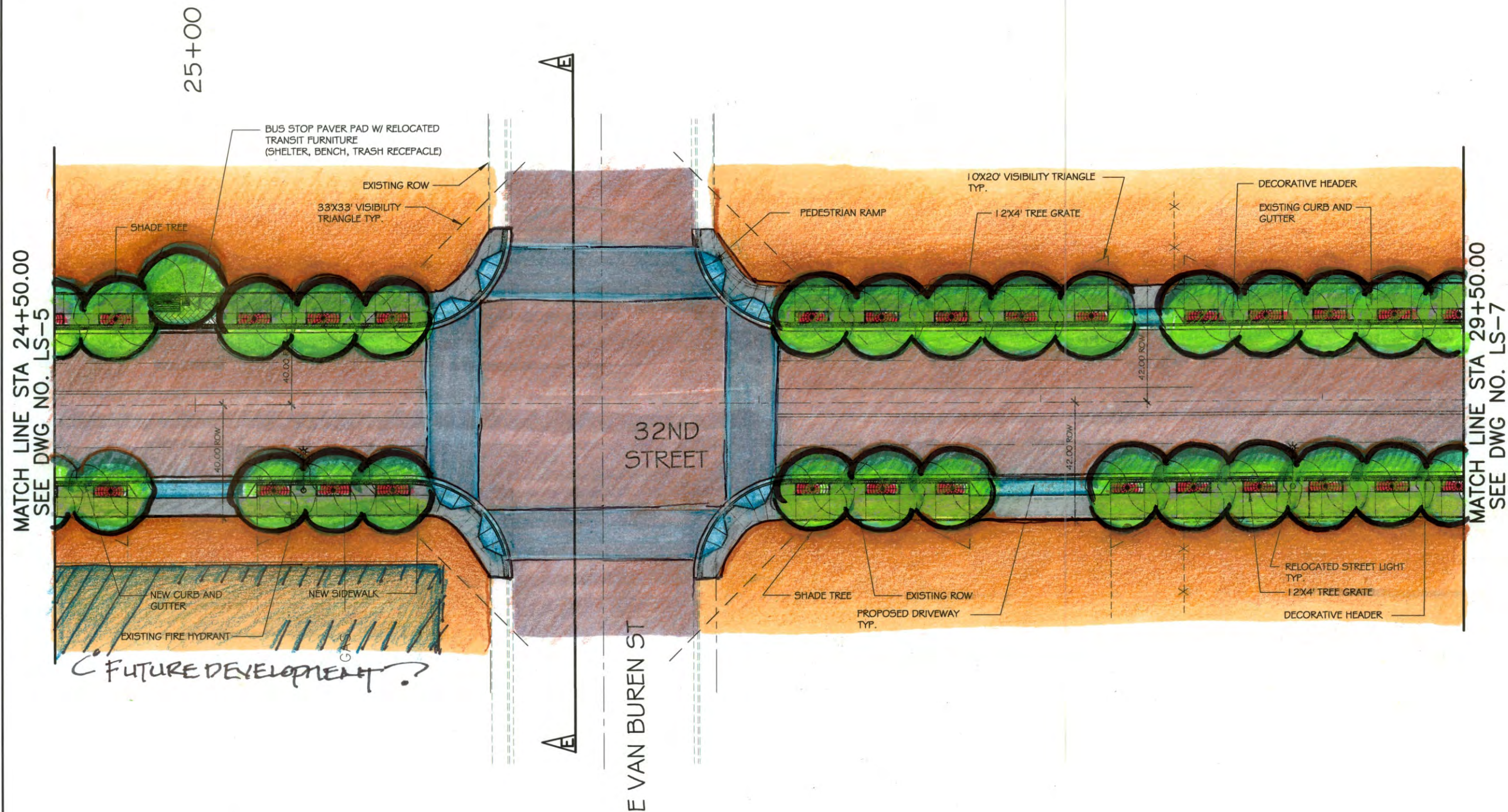
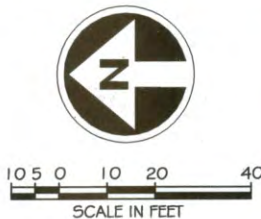


SHEET LS-5  
CONCEPTUAL LANDSCAPE PLAN  
32ND STREET  
STA 19+50.00 TO STA 24+50.00

SHERMAN GROUP INC.  
8817 N Central Ave  
Phoenix, AZ 85020  
PH: 602.216.0322  
FX: 602.216.2772

MARICOPA ASSOCIATION OF GOVERNMENTS





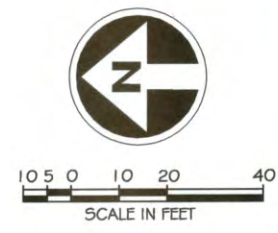
NOTE:  
ALL TREES IN VISIBILITY TRIANGLES TO BE PRUNED TO ALLOW A MINIMUM OF 6' 8" CLEARANCE BELOW THE CANOPY  
SEE CROSS SECTIONS FOR LANE WIDTH, SIDEWALK AND PLANTER DIMENSIONS.

NOTE:  
FOR SECTIONS SEE SHEET LS-9



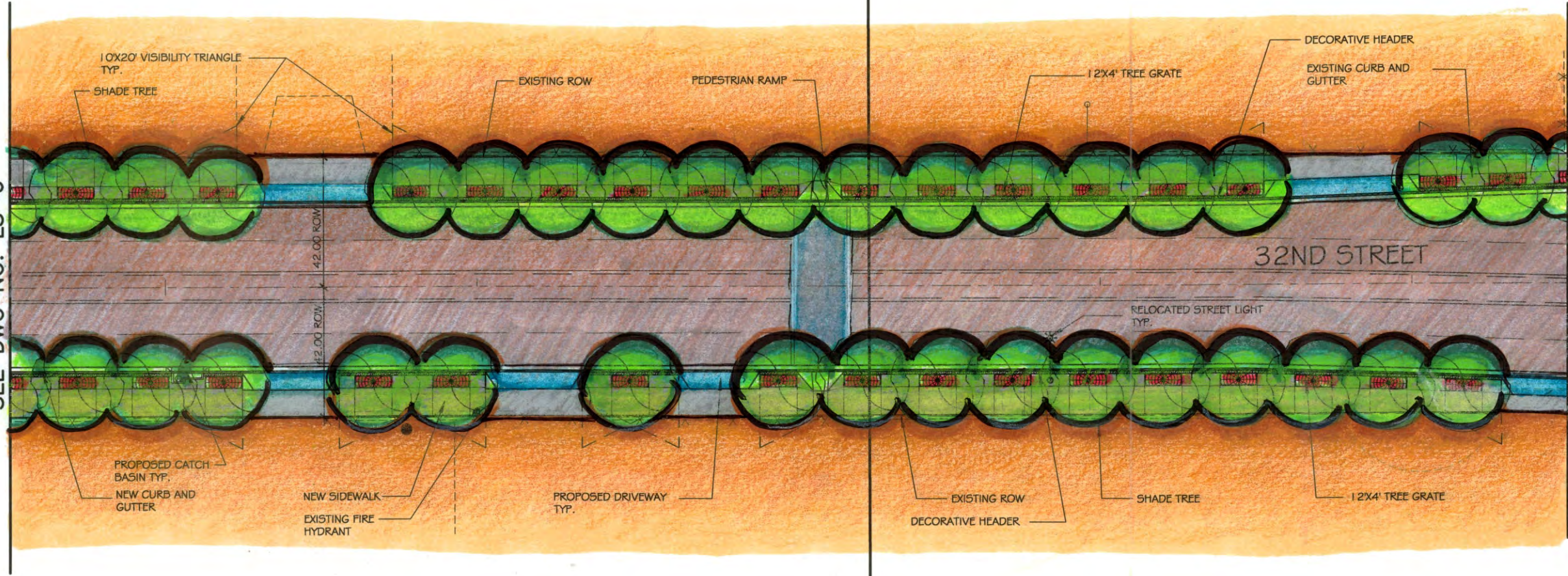
SHEET LS-6  
CONCEPTUAL LANDSCAPE PLAN  
32ND STREET  
STA 24+50.00 TO STA 29+50.00





MATCH LINE STA 29+50.00  
SEE DWG NO. LS-6

30+00



MATCH LINE STA 34+50.00  
SEE DWG NO. LS-8

NOTE:  
ALL TREES IN VISIBILITY TRIANGLES TO BE PRUNED TO ALLOW A MINIMUM OF 6' 8" CLEARANCE BELOW THE CANOPY  
SEE CROSS SECTIONS FOR LANE WIDTH, SIDEWALK AND PLANTER DIMENSIONS.

NOTE:  
FOR SECTIONS SEE SHEET LS-9

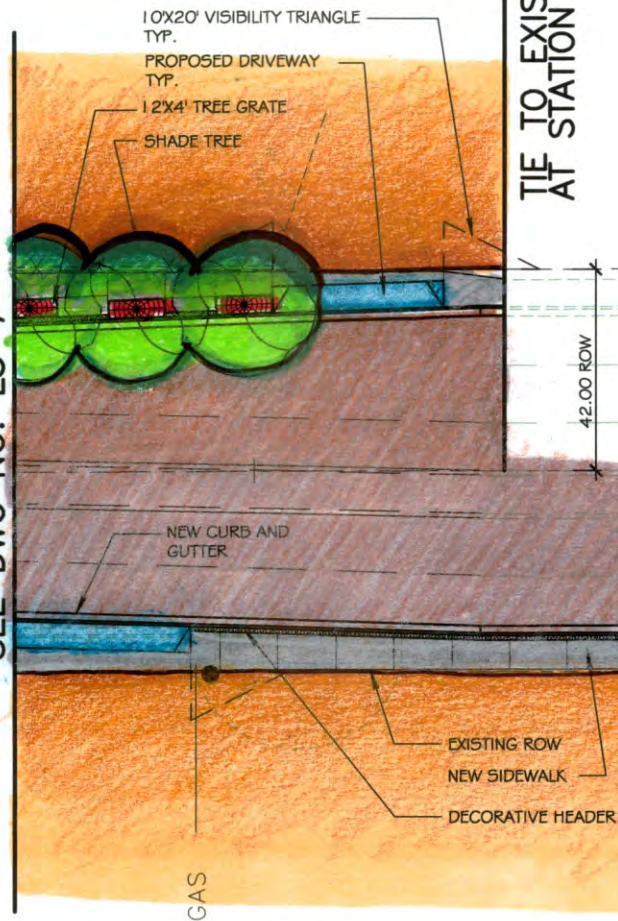


SHEET LS-7  
CONCEPTUAL LANDSCAPE PLAN  
32ND STREET  
STA 29+50.00 TO STA 34+50.00



MATCH LINE STA 34+50.00  
SEE DWG NO. LS-7

35+00



TIE TO EXISTING  
AT STATION 35+52.11

TIE TO EXISTING  
AT STATION 35+94.70

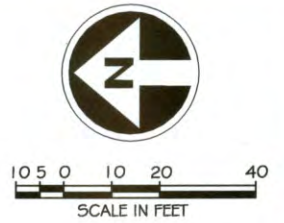
PEDESTRIAN RAMP  
EXISTING CURB AND  
GUTTER  
EXISTING ROW

42.00 ROW

E WASHINGTON ST

32ND STREET

LIMITS OF PROJECT



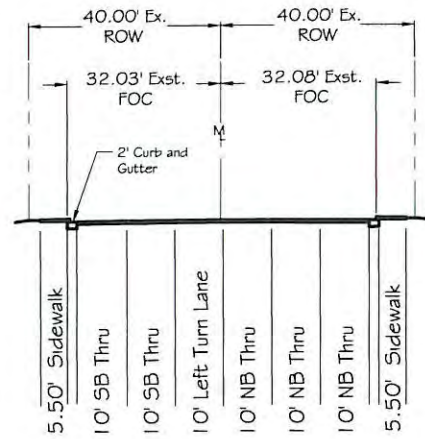
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NOTE:  
FOR SECTIONS SEE SHEET LS-9

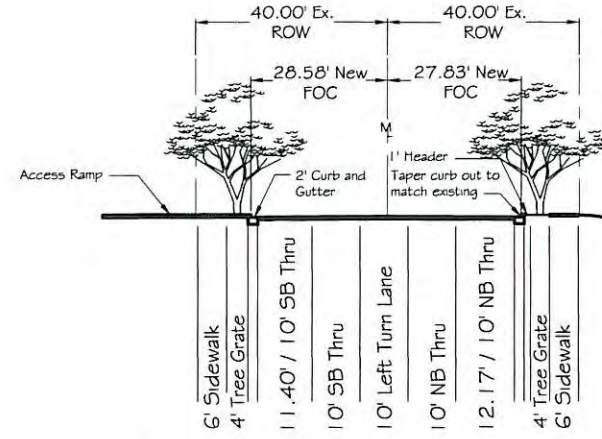


SHEET LS-8  
CONCEPTUAL LANDSCAPE PLAN  
32ND STREET  
STA 34+50.00 TO LIMITS

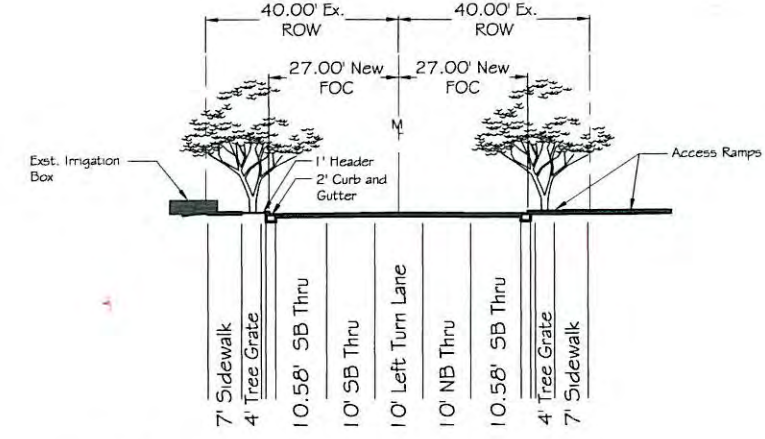




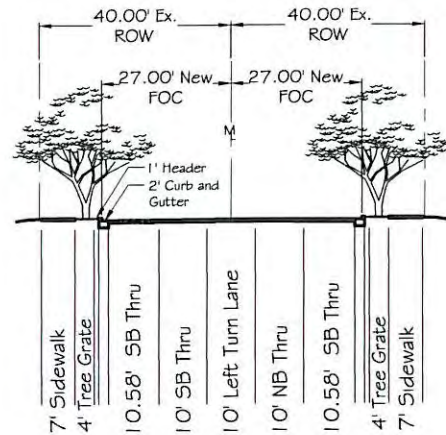
A SECTION A-A  
A E. ROOSEVELT ST



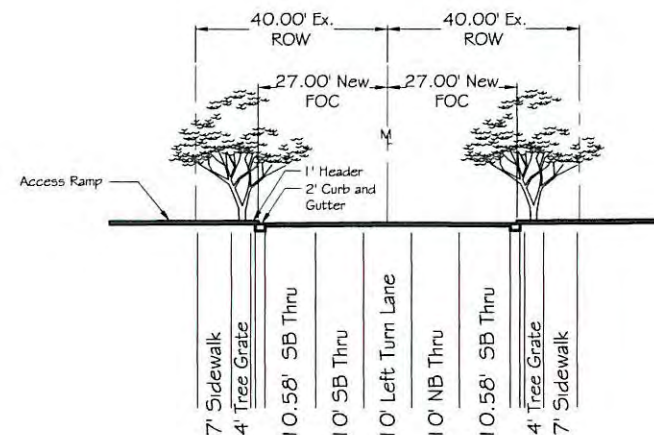
B SECTION B-B  
B E. GARFIELD ST



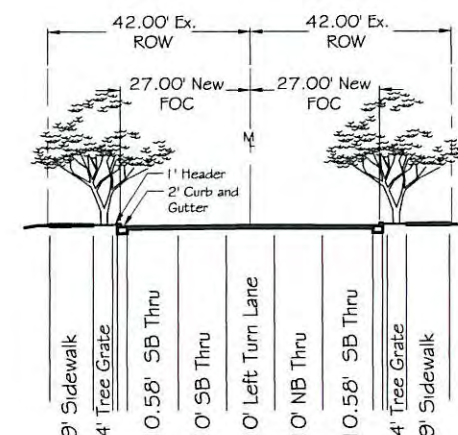
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C N. RANDOLPH RD.



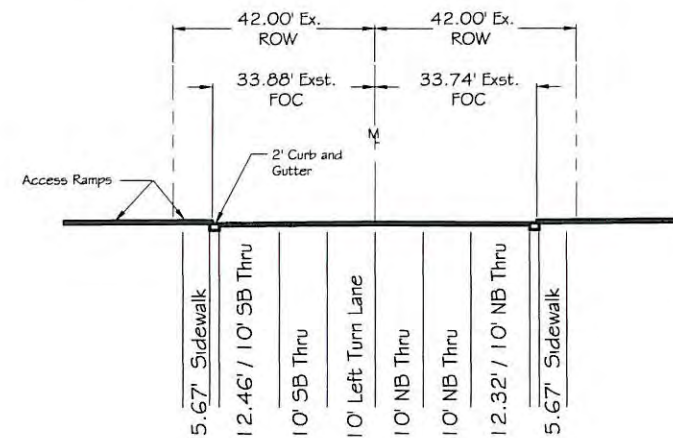
D SECTION D-D  
D E. FILMORE ST. - VAN BUREN ST



E SECTION E-E  
E E. VAN BUREN ST



F SECTION F-F  
F E. VAN BUREN ST - E. WASHINGTON ST



G SECTION G-G  
G E. WASHINGTON ST



SHEET LS-9  
CROSSSECTION DETAILS  
32ND STREET



32nd Street Conceptual Cost Estimate 1

#		Description	Unit	Unit Price	Total
1	1	Mobilization	LS		\$115,874.46
2	1	Misc. Removals	LS		\$40,234.19
3	1	Traffic Control	LS		\$13,411.40
4	1	Site Grading	LS		\$53,645.58
5	480	Off Duty Police Officer	HR	\$40.00	\$19,200.00
6	6661	Sawcut and Remove Existing Concrete Curb and Gutter	LF	\$5.00	\$33,305.00
7	12550	Sawcut and Remove Existing Concrete Slabs and/or Drives	SF	\$3.00	\$37,650.00
8	28500	Sawcut and Remove Existing Concrete Sidewalk	SF	\$3.00	\$85,500.00
9	6,663	Saw Cut and Remove Existing Asphaltic Concrete Pavement	SY	\$6.50	\$43,309.50
10	21	Remove and Relocate Existing Water Meter, Box and Cover	EA	\$300.00	\$6,300.00
11	3	Remove and Relocate Existing Fire Hydrant	EA	\$800.00	\$2,400.00
12	8	Remove Existing Catch Basin and Attached Pipe Sections as an Incidental	EA	\$900.00	\$7,200.00
13	7	Remove and Relocate Existing Stop signs and Associated Street Nomenclature	EA	\$120.00	\$840.00

14	17	Remove and Relocate Existing Traffic Control Signs	EA	\$120.00	\$2,040.00
15		Obliterate Existing Striping	LF	\$2.00	\$46,746.00
16	5	Remove Existing Tree	EA	\$500.00	\$2,500.00
17	5	Remove Existing Palm	EA	\$700.00	\$3,500.00
18	109	Remove and Reconstruct Existing Block Wall - Finish to Match Previous Treatment	LF	\$50.00	\$5,450.00
19	13	Remove and Relocate Existing Street Lights	EA	\$1,000.00	\$13,000.00
20	4	Remove and Relocate Existing Traffic Signal / No Mast Arm	EA	\$1,500.00	\$6,000.00
21	4	Remove and Relocate Existing Traffic Signal / Mast Arm	EA	\$3,000.00	\$12,000.00
22	1	Remove and Relocate Existing Traffic Control Cabinet	EA	\$1,500.00	\$1,500.00
23	8	Adjust Manhole Frame and Cover to New Grade Per MAG STD Detail 240	EA	\$400.00	\$3,200.00
24	20	Adjust Frame and Cover for (Water Valve, Survey Monuments or Sewer Cleanouts) to New Grade per MAG STD Detail 270	EA	\$300.00	\$6,000.00
25	6760	Install Curb and Gutter per MAG	LF	\$8.00	\$54,080.00
26	22	Install Concrete curb transition	EA	\$60.00	\$1,320.00
27	32	Install New Handicap Access Ramp Per Civil Details	EA	\$500.00	\$16,000.00



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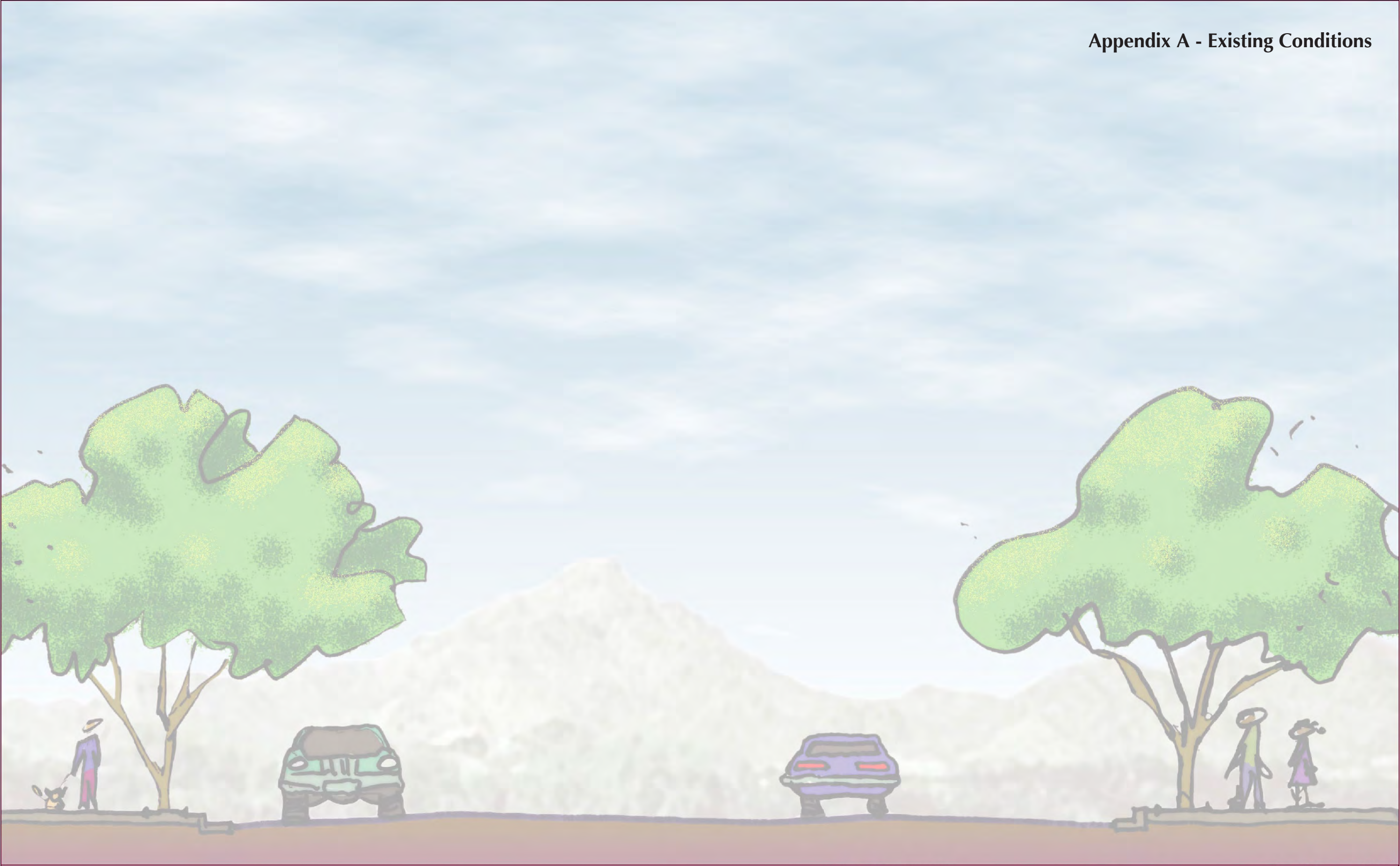


28	54900	Install New Salt Finish	SF	\$4.50	\$247,050.00
		Concrete sidewalk per Plans and Det.			
29	5590	Install Paver Surface as per Plans and Det.	SF	\$7.50	\$41,925.00
30	5785	Install New Driveway as per MAG	SF	\$4.50	\$26,032.50
		Standard Detail 251 to Size Indicated			
31	4	Install New Catch Basin Single Type "E"	EA	\$2,500.00	\$10,000.00
		MAG 534			
32	4	Install New Catch Basin Double Type "E"	EA	\$4,500.00	\$18,000.00
		MAG 534			
33	2	Remove and Reinstall as Indicated on Plans	EA	\$250.00	\$500.00
		Transit Bench			
34	2	Remove and Reinstall as Indicated on Plans	EA	\$150.00	\$300.00
		Transit Trash Receptacle			
35	2	Remove and Reinstall as Indicated on Plans	EA	\$600.00	\$1,200.00
		Transit Shelter			
36	3203	Subgrade Prep for AC Pavement	SY	\$8.00	\$25,624.00
37	3203	4" Thick Aggregate Base Course	SY	\$5.50	\$17,616.50
38	3203	2" Thick Asphaltic Concrete Pavement	SY	\$5.00	\$16,015.00
		Base Course			
39	3203	1" Thick Asphaltic Concrete Pavement	SY	\$4.00	\$12,812.00
		Surface Course			
40	1799	12" Wide White Solid Stripe	LF	\$3.00	\$5,397.00
41	527	18" Wide White Solid Stripe	LF	\$4.50	\$2,371.50
42	5276	8" Wide White Skip Stripe	LF	\$1.50	\$7,914.00
43	5276	4" Wide Yellow Skip Stripe	LF	\$0.80	\$4,220.80

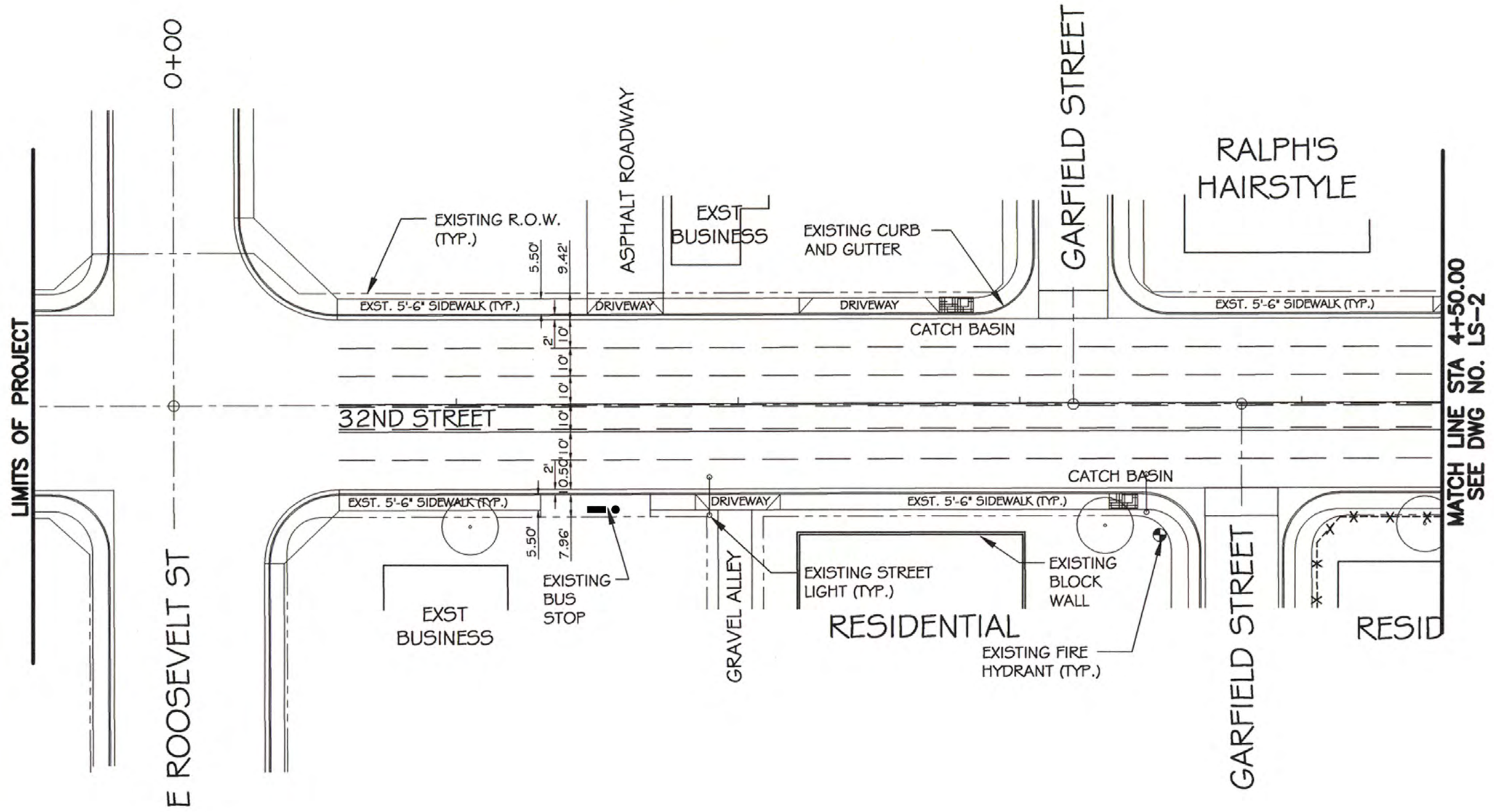
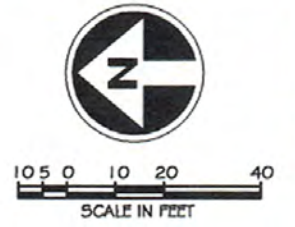
44	5276	4" Wide Yellow Solid Stripe	LF	\$0.80	\$4,220.80
45	6	Preformed Painted Left Turn Arrow	LF	\$300.00	\$1,800.00
46	18	Preformed Painted Reverse Lane	LF	\$550.00	\$9,900.00
		Turn Arrows			
47	8	Sign and Pole, Complete and Installed	LF	\$250.00	\$2,000.00
		(Ped/Crossing)			
48	200	Tree Grate Installed	LF	\$900.00	\$180,000.00
49	203	48" Box Trees	EA	\$1,200.00	\$243,600.00
50	60	Decomposed Granite	CY	\$60.00	\$3,600.00
51	2	1" Water Meter Service, Including	EA	\$4,000.00	\$8,000.00
		all Pipe, Fittings, Trenching and			
		Connection to Water Supply			
52	2	1" Reduced Pressure Backflow	EA	\$2,000.00	\$4,000.00
		Prevention Unit and Cage			
53	2	Irrigation Controller	EA	\$1,500.00	\$3,000.00
54	1	Irrigation System Complete	LS	\$35,000.00	\$35,000.00
		Grand Total			\$1,564,305.23











MATCH LINE STA 4+50.00  
SEE DWG NO. LS-2

CALL TWO WORKING DAYS  
BEFORE YOU DIG  
602-233-1100  
1-800-STAKE-IT  
OUTSIDE MARICOPA COUNTY

EXISTING SIE PLAN  
32ND STREET  
STA 0+50.00 TO STA 4+50.00

SHERMAN GROUP INC.  
8837 N Central Ave  
Phoenix, AZ 85020  
PH: 602.216.2772  
FX: 602.216.2772

MARICOPA ASSOCIATION OF GOVERNMENTS

32<sup>ND</sup> Washington Pedestrian Project

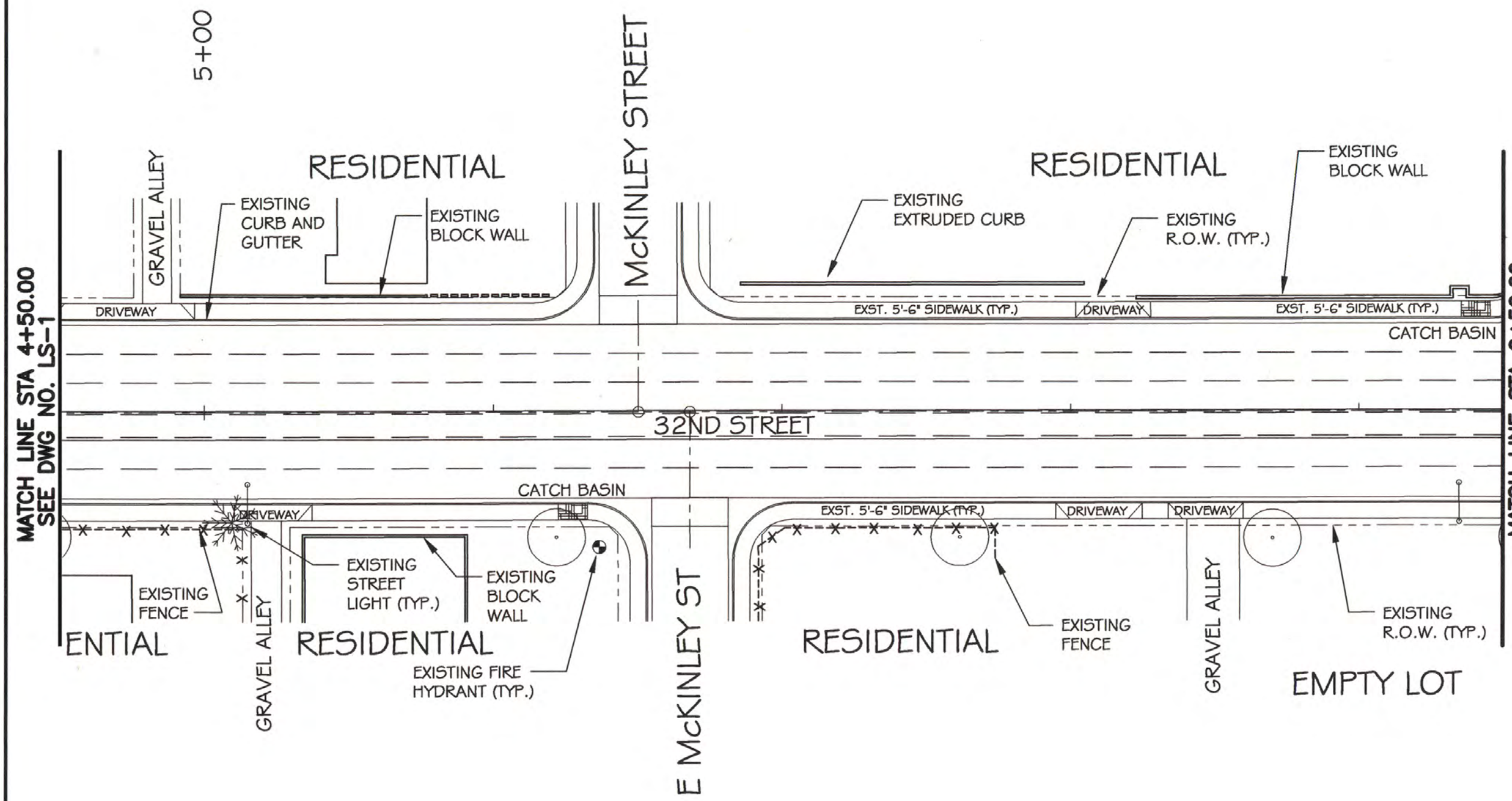




10 5 0 10 20 40  
SCALE IN FEET

MATCH LINE STA 4+50.00  
SEE DWG NO. LS-1

MATCH LINE STA 9+50.00  
SEE DWG NO. LS-3



CALL TWO WORKING DAYS  
BEFORE YOU DIG  
602-263-1100  
1-800-STAKE-IT  
OUTSIDE MARICOPA COUNTY

EXISTING SITE PLAN  
32ND STREET  
STA 4+50.00 TO STA 9+50.00

**32ND Washington Pedestrian Project**

**SHERMAN GROUP INC**  
8037 N Central Ave  
Phoenix, AZ 85020  
Ph: 602.216.2022  
F: 602.216.2772

MARICOPA ASSOCIATES OF GOVERNMENTS



MATCH LINE STA 9+50.00  
SEE DWG NO. LS-2

E. PIERCE

10+00

RESIDENTIAL

SHED

EXISTING  
WOOD FENCE

EXISTING  
CURB AND  
GUTTER

N. RANDOLPH RD.

EXISTING  
CONCRETE  
CURB (TYP.)

EXISTING BUS  
STOP SIGN

EXISTING R.O.W.  
(TYP.)

EXISTING  
R.O.W. (TYP.)

EXISTING  
BLOCK WALL

DRIVEWAY

EXIST. 5'-6" SIDEWALK (TYP.)

32ND STREET

EXIST. 5'-6" SIDEWALK (TYP.)

EXISTING FIRE  
HYDRANT (TYP.)

ABANDONED SALT  
RIVER PROJECT  
CANAL

EXISTING  
IRRIGATION BOX

EXISTING  
R.O.W. (TYP.)

DRIVEWAY

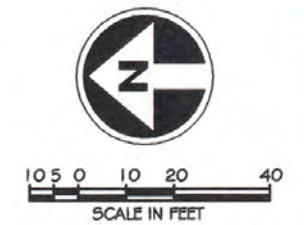
CATCH BASIN

EMPTY LOT

EXISTING CHIAN  
LINK FENCE

FILLMORE STREET

MATCH LINE STA 14+50.00  
SEE DWG NO. LS-4



EXISTING SITE PLAN  
32ND STREET  
STA 9+50.00 TO STA 14+50.00





10 5 0 10 20 40  
SCALE IN FEET

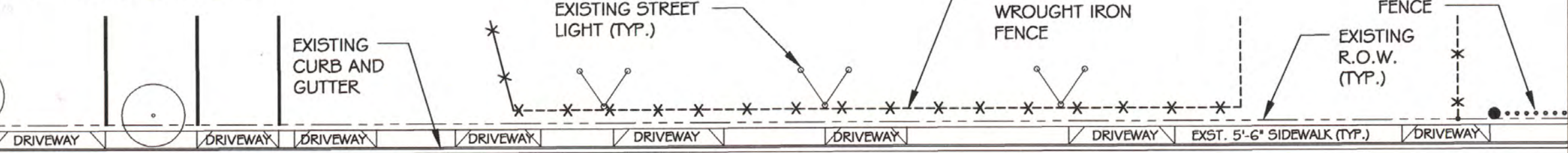
MATCH LINE STA 14+50.00  
SEE DWG NO. LS-3

MATCH LINE STA 19+50.00  
SEE DWG NO. LS-5

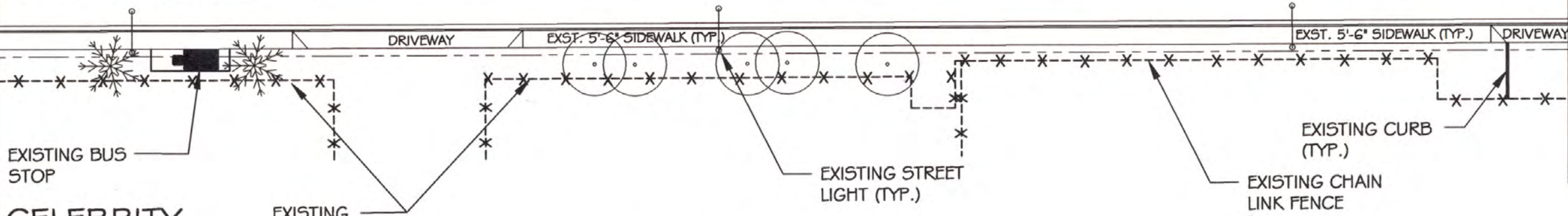
15+00  
CYPRESS POINT  
APARTMENTS

EDDIE'S USED  
CARS

EMPTY LOT



32ND STREET



CELEBRITY  
THEATER

CELEBRITY  
THEATER

HAZELWOOD

CALL TWO WORKING DAYS  
BEFORE YOU DIG  
**602-263-1100**  
**1-800-STAKE-IT**  
OUTSIDE MARICOPA COUNTY

EXISTING SITE PLAN  
32ND STREET  
STA 14+50.00 TO STA 19+50.00



SHERMAN  
GROUP INC  
8837 N Central Ave  
Phoenix, AZ 85020  
PH: 602-216-3022  
FX: 602-216-2772



32ND/Washington Pedestrian Project





10 5 0 10 20 40  
SCALE IN FEET

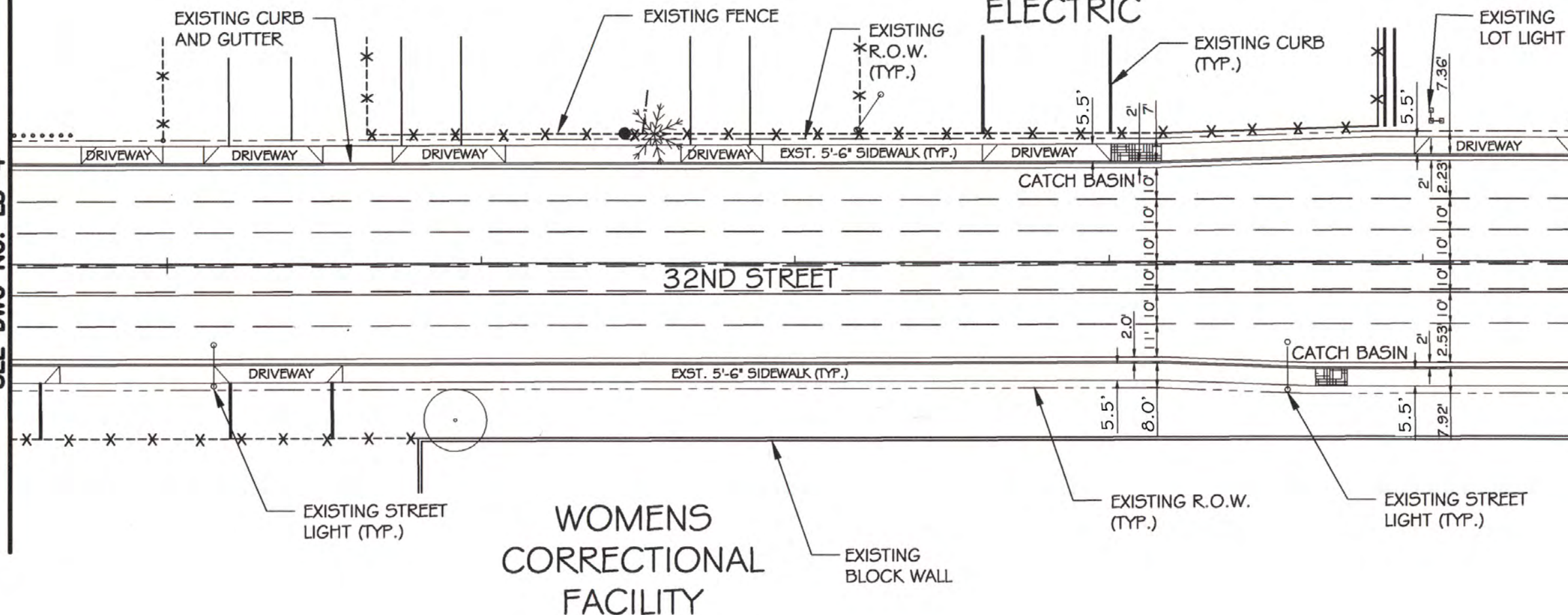
MATCH LINE STA 19+50.00  
SEE DWG NO. LS-4

20+00

BAJITO  
UPHOLSTERY

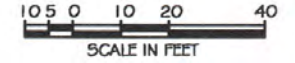
HILLCREST  
ELECTRIC

MATCH LINE STA 24+50.00  
SEE DWG NO. LS-6



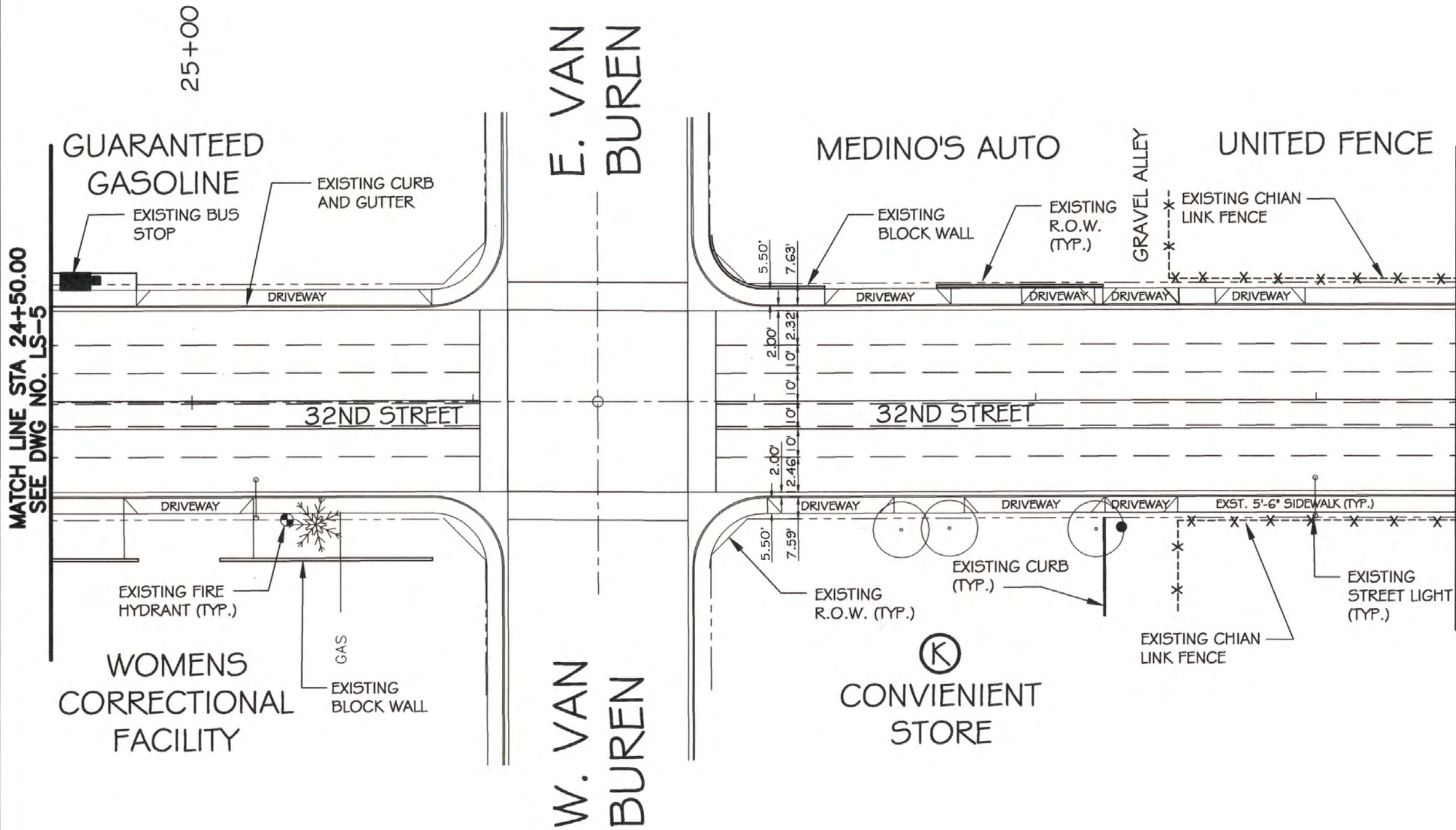
EXISTING SITE PLAN  
32ND STREET  
STA 19+50.00 TO STA 24+50.00





MATCH LINE STA 24+50.00  
SEE DWG NO. LS-5

MATCH LINE STA 29+50.00  
SEE DWG NO. LS-7



CALL TWO WORKING DAYS  
BEFORE YOU DIG  
**602-263-1100**  
**1-800-STAKE-IT**  
OUTSIDE MARICOPA COUNTY

EXISTING SITE PLAN  
32ND STREET  
STA 24+50.00 TO STA 29+50.00

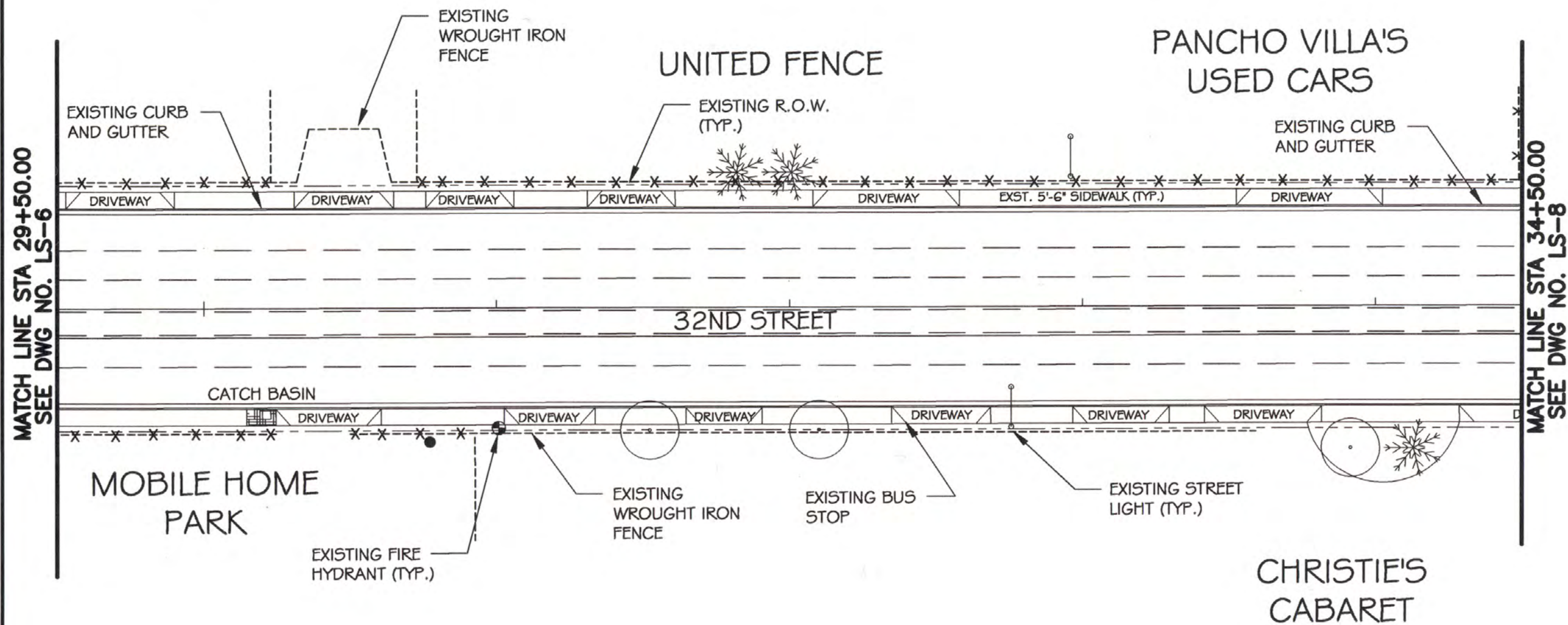
**32ND Washington Pedestrian Project**

**SHERMAN GROUP INC**  
8937 N Central Ave  
Phoenix, AZ 85020  
Ph: 602.216.2022  
Fax: 602.216.2772





10 5 0 10 20 40  
SCALE IN FEET



MATCH LINE STA 29+50.00  
SEE DWG NO. LS-6

MATCH LINE STA 34+50.00  
SEE DWG NO. LS-8



EXISTING SITE PLAN  
32ND STREET  
STA 29+50.00 TO STA 34+50.00

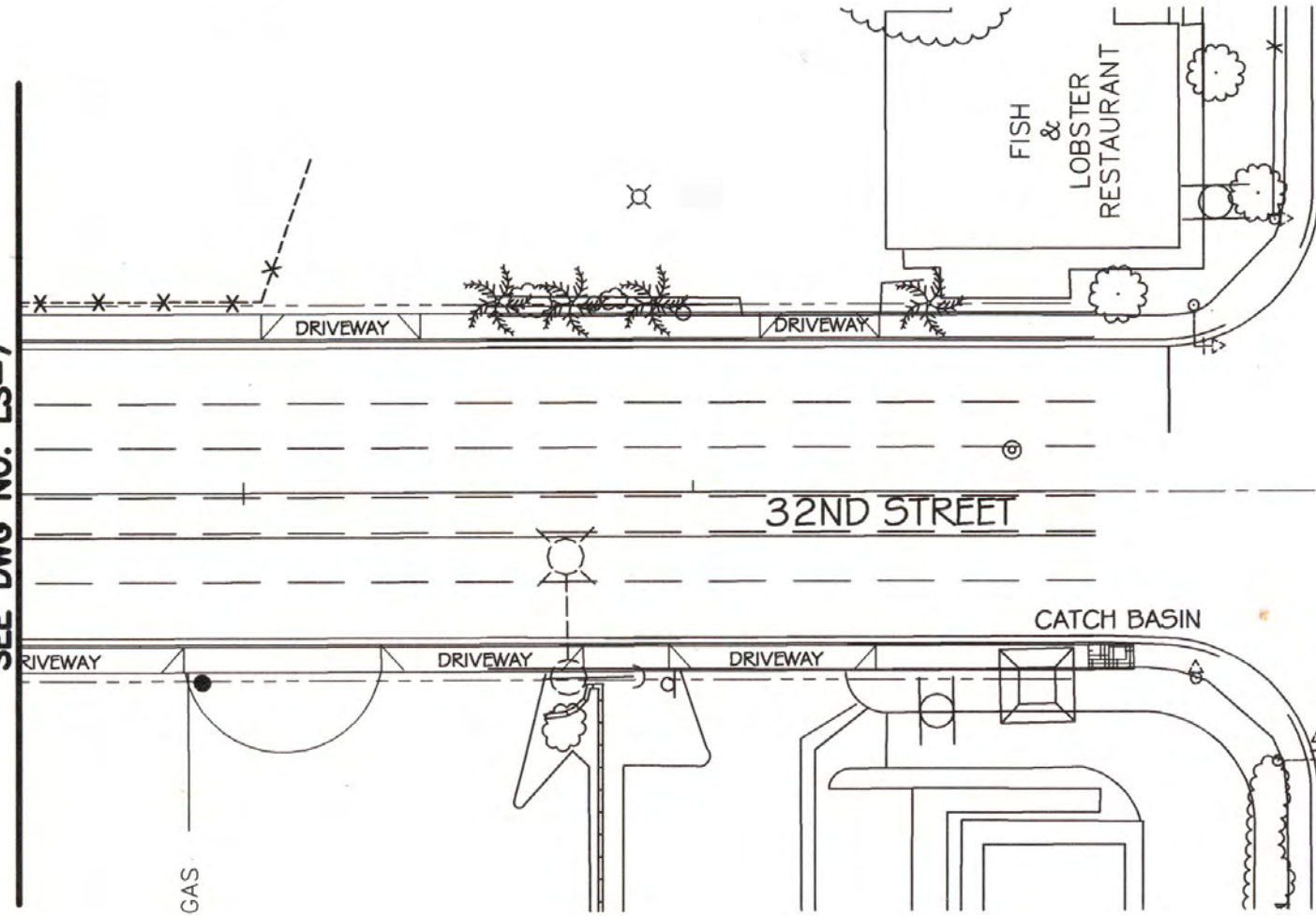


32<sup>ND</sup> Washington Pedestrian Project

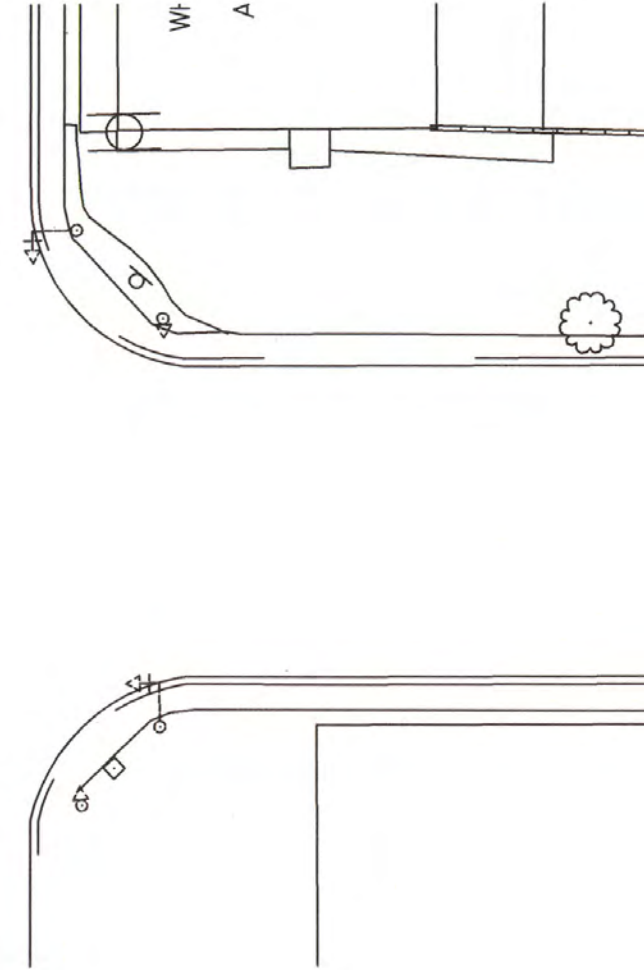


MATCH LINE STA 34+50.00  
SEE DWG NO. LS-7

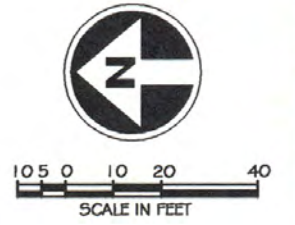
35+00



WASH  
WASHINGTON STREET



LIMITS OF PROJECT



EXISTING SITE PLAN  
32ND STREET  
STA 34+50.00 TO LIMITS

32ND/Washington Pedestrian Project

SHERMAN  
GROUP INC  
6037 N Central Ave  
Phoenix, AZ 85020  
PH: 602.216.3022  
FX: 602.216.2772

